



**FILED**

7-13-17  
08:00 AM

**BEFORE THE PUBLIC UTILITIES COMMISSION  
OF THE STATE OF CALIFORNIA**

Application of the CITY OF SAN MATEO for  
an Order Authorizing Construction of two new  
grade separated vehicular underpasses and one  
pedestrian underpass at PCJPB MPs 19.92, 20.15  
and 20.03 (DOT No. 973 108 V, 973 109C and  
973 110W) in the City of San Mateo in the  
County of San Mateo

A1707006

) Application No. \_\_\_\_\_

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**APPLICATION**

1. The City of San Mateo, hereinafter referred to as “CITY,” respectfully requests authority from this Commission to construct two new grade separated pedestrian/vehicular underpasses and one new grade separated pedestrian underpass owned and operated by the Peninsula Corridor Joint Powers Board (PCJPB) in the City of San Mateo in the County of San Mateo at PCJPB Mileposts 19.92, 20.15, and 20.03 (DOT No. 973 108 V, 973 109C and 973 110W). The new underpasses are located at 28<sup>th</sup> Avenue, 31<sup>st</sup> Avenue and the new Hillsdale Station, respectively. The CITY has been pursuing the development of the 25<sup>th</sup> Avenue Grade Separation project, which includes these three new crossings as well as grade separating the existing at-grade crossing at 25<sup>th</sup> Avenue, through the cooperative efforts of many funding agencies.

2. The Peninsula Corridor Joint Powers Board, herein referred to as “PCJPB”, is a public agency providing commuter train services that owns and operates a standard gauge railroad system in the Counties of San Francisco, San Mateo, and Santa Clara, in the State of California and said public agency owns and operates railroad tracks at the location of the project herein proposed to be constructed. The railroad corridor at this location has two

tracks that carry over ninety (90) passenger trains per day for both tracks. The approximate speed for the commuter trains is 79 mph.

3. All correspondence, communication, notices, orders and other papers relative to this application should be addressed to:

City of San Mateo  
Attention: Brad Underwood, Director of Public Works  
300 W. 20<sup>th</sup> Avenue  
San Mateo, CA 94403  
Telephone: 650-522-7300  
Fax: 650-522-7301  
Email: [bunderwood@cityofsanmateo.org](mailto:bunderwood@cityofsanmateo.org)

Pursuant to Rule 2.1(b), the CITY consents to e-mail service at:

[bunderwood@cityofsanmateo.org](mailto:bunderwood@cityofsanmateo.org)

4. Copies of such correspondence, notices, orders and other papers should also be addressed to the PCJPB:

Peninsula Corridor Joint Powers Board  
Attention: Rafael Bolon, Acting Deputy Director – Capital Projects  
1250 San Carlos Avenue, 4<sup>th</sup> Floor  
San Carlos, CA 94070  
Telephone: 650-622-7805  
Fax: 650-508-7938  
Email: [bolonr@samtrans.com](mailto:bolonr@samtrans.com)

5. This application and relief sought herein is filed pursuant to Sections 1201 through 1205, inclusive, of the Public Utilities Code of the State of California and is made in accordance Rule 3.7 of the California Public Utilities Commission Rules of Practice and Procedure.

6. The 25th Avenue Grade Separation project includes construction of approximately one (1) mile of new railroad grade-separation, from Hillsdale Avenue to SR-92, by partially raising the rails in a lightweight-cellular-concrete (LCC) filled MSE wall, partially

depressing the existing road at 25th Avenue, and constructing two (2) new road connections at 28th and 31st Avenues. The grade crossings require the installation of two (2) new concrete precast bridges and one (1) steel bridge. The bridges shall be supported by CIDH and cast in place abutments and center columns. In addition, the project will construct a new elevated, center-board, Caltrain station, with access from 28th avenue, as well as from a two-cell cast-in-place pedestrian viaduct. The project will also include grading and drainage improvement, changes to existing traffic lights, additional street and platform lighting, relocation of existing rail signals, and custom made shelters and other amenities at the station.

7. Environmental resources will be protected during and after the construction. Much of the proposed track alignment will be located parallel to the existing railroad tracks at the edge of the railroad right-of way. As set forth in the project plans accompanying this application, horizontal and vertical clearances from the railroad tracks will comply with CPUC General Order 26-D.

8. The nearest public crossings along the railroad tracks are:

- 105E-19.52, DOT # 754910E: 25<sup>th</sup> Avenue At-Grade Crossing in City of San Mateo
- 105E-20.06-D, DOT # 754912T: Hillsdale Station North Pedestrian Crossing in City of San Mateo

9. The project was advertised in December 2016 with construction anticipated to commence late summer in 2017. The construction will be completed within three years.

10. The following exhibits are transmitted in support of the present application:

- Location and Vicinity Maps attached hereto as **Exhibit A**;

- General Plans including Plan, Elevation and Sections as **Exhibits B**;
- Legal Description, attached hereto as **Exhibit C**; and
- Memo regarding CEQA exemption, attached hereto as **Exhibit D**.

11. Final plans for the proposed project were prepared by PCJPB. The CITY and PCJPB are in agreement with respect to the necessity for said construction. Copies of the final plans approved by PCJPB, the "Construction and Maintenance" and the "Right-of-Way" Agreements will be provided separately to the Safety and Enforcement Division staff in San Francisco. The City has secured funding for this project under Measure A, City, State Section 190, and State HSR Prop 1A funds.

12. The City is the lead agency for this project. The project has been developed so as to give detailed consideration to the potential impact upon the quality of the environment. The PCJPB has determined the 25th Avenue Grade Separation Project to be statutorily exempt under Public Resources Code Section 21080.13 which provides exemption from CEQA.

Dated at San Mateo, California, this 11 date of July, 2017

CITY OF SAN MATEO

By:   
Brad Underwood, Director of Public Works

**VERIFICATION**  
(CPUC Rule 1.11)

**For Applicant City of San Mateo:**

I, Brad Underwood, hereby declare that I am the Director of Public Works for the City of San Mateo; that I am authorized to make this verification on behalf of said Agency and that I have read the foregoing Application and know the contents thereof. The statements in the foregoing Application are true of my own knowledge, except as to the matters that are herein stated on information or belief, as to those matters I believe them to be true.

I declare under penalty of perjury that the foregoing is true and correct.

Executed at San Mateo, California on July 11, 2017



\_\_\_\_\_  
Brad Underwood / Director of Public Works  
City of San Mateo

## EXHIBIT A

### Location and Vicinity Map



Vicinity Map





## EXHIBIT B

### General Plans



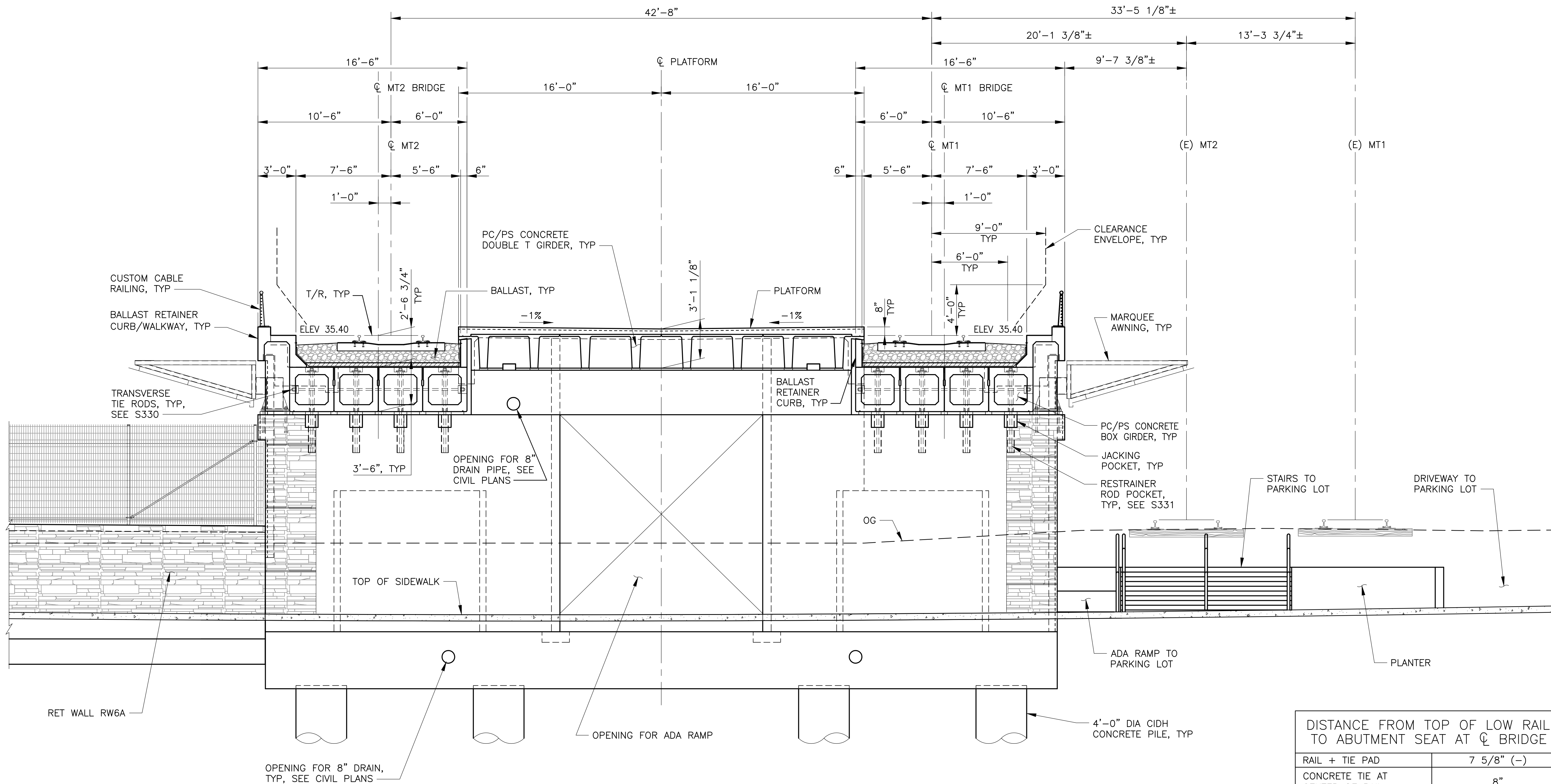


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COUNTY:

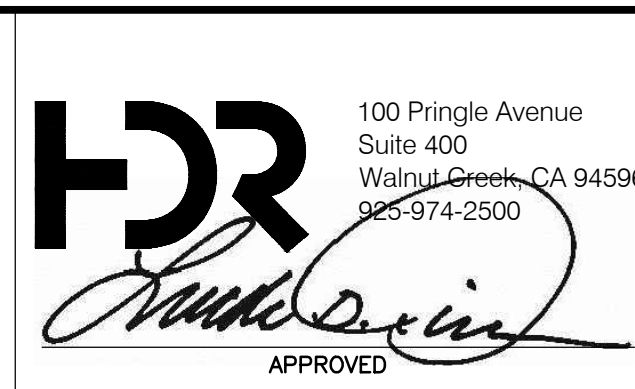
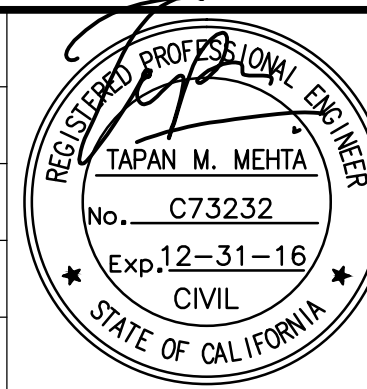


(LOOKING NORTH)  
**TYPICAL SECTION AT ABUTMENT 1**  
SCALE: 1/4" = 1'-0"

DISTANCE FROM TOP OF LOW RAIL TO ABUTMENT SEAT AT CL BRIDGE	
RAIL + TIE PAD	7 5/8" (-)
CONCRETE TIE AT CENTER OF RAIL	8"
MIN BALLAST	10 1/8"
HMAC	4"
WATERPROOFING	1" (+)
PC/PS CONC BOX GIRDER	42" (3'-6")
BEARINGS	3"
TOTAL	75 3/4" (6'-3 3/4")

REV	DATE	BY	SUB	APP	DESCRIPTION
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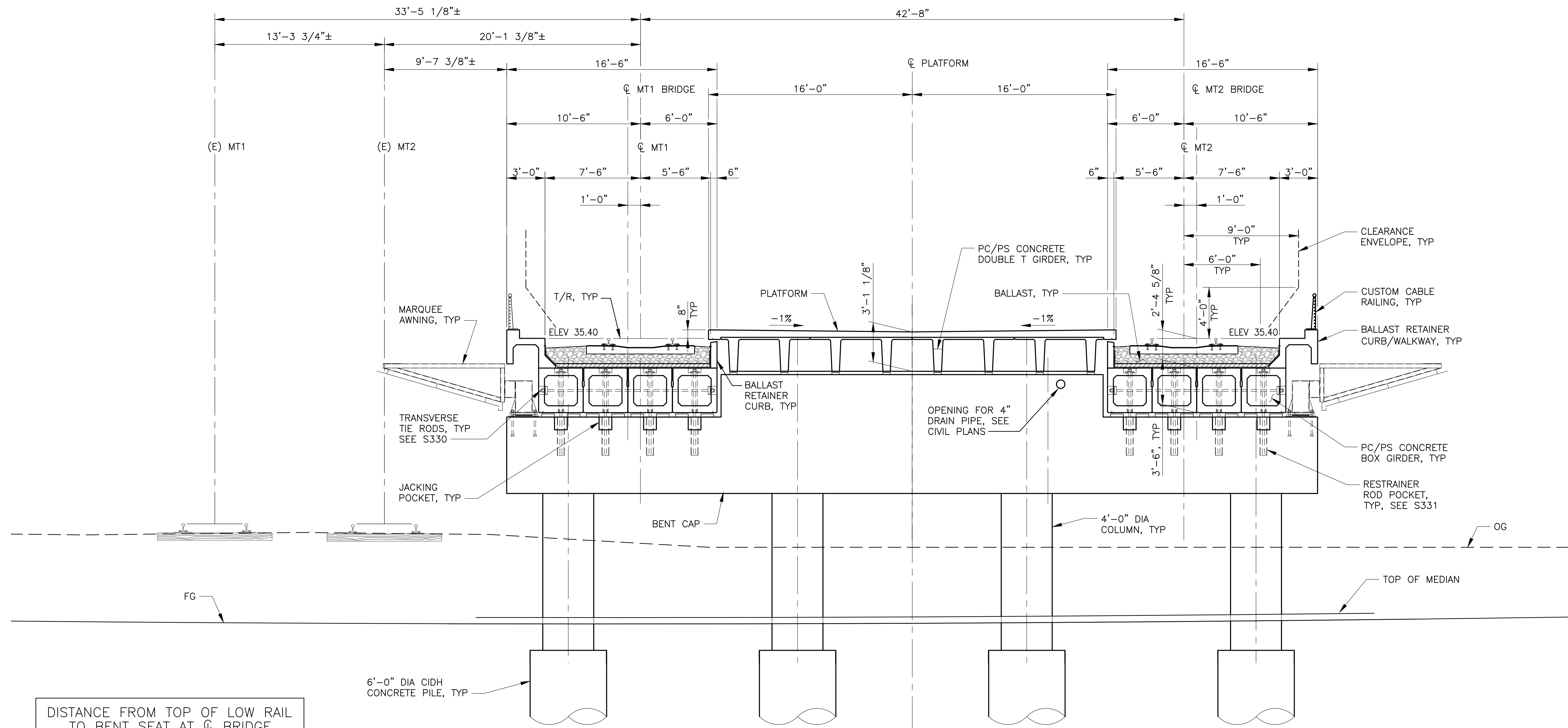
DESIGNED  
T. MEHTA  
DRAWN  
J. VOUGHT  
CHECKED  
J. SUK  
IN CHARGE  
L. RIMBACH  
DATE  
12092016



DEPUTY DIRECTOR  
S. CHAO  
PROJECT MANAGEMENT  
R. BOLON  
STRUCTURES  
C. CHEN  
TRACK/CIVIL  
S. CHAO

PENINSULA CORRIDOR JOINT POWERS BOARD	
SAN MATEO 25TH AVENUE GRADE SEPARATION BR 19.92 OVER 28TH AVENUE TYPICAL SECTION AT ABUTMENT 1	
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SCALE AS SHOWN	
CONTRACT NO 17-J-C-048	MILEPOST 19.0-20.3
DWG NO S301	REV 0
	PAGE NO 436

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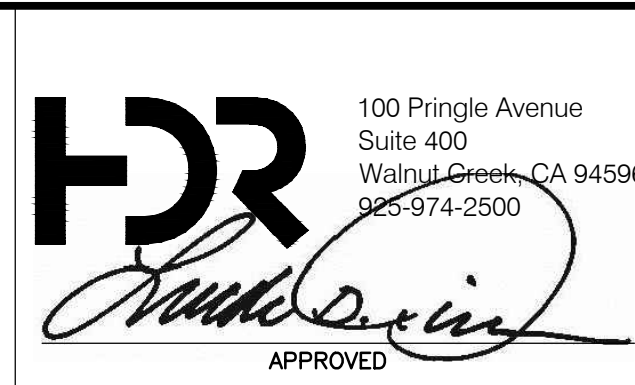
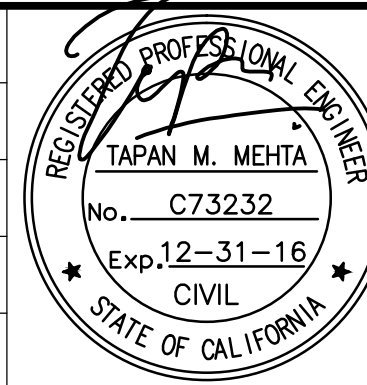


DISTANCE FROM TOP OF LOW RAIL TO BENT SEAT AT CL BRIDGE	
RAIL + TIE PAD	7 5/8" (-)
CONCRETE TIE AT CENTER OF RAIL	8"
MIN BALLAST	8"
HMAC	4"
WATERPROOFING	1" (+)
PC/PS CONC BOX GIRDER	42" (3'-6")
BEARINGS	3"
TOTAL	75 5/8" (6'-1 5/8")

(LOOKING SOUTH)  
**TYPICAL SECTION AT BENT 2**  
SCALE: 1/4" = 1'-0"

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DESIGNED  
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L. RIMBACH  
DATE  
12092016



DEPUTY DIRECTOR  
S. CHAO  
PROJECT MANAGEMENT  
R. BOLON  
STRUCTURES  
C. CHEN  
TRACK/CIVIL  
S. CHAO

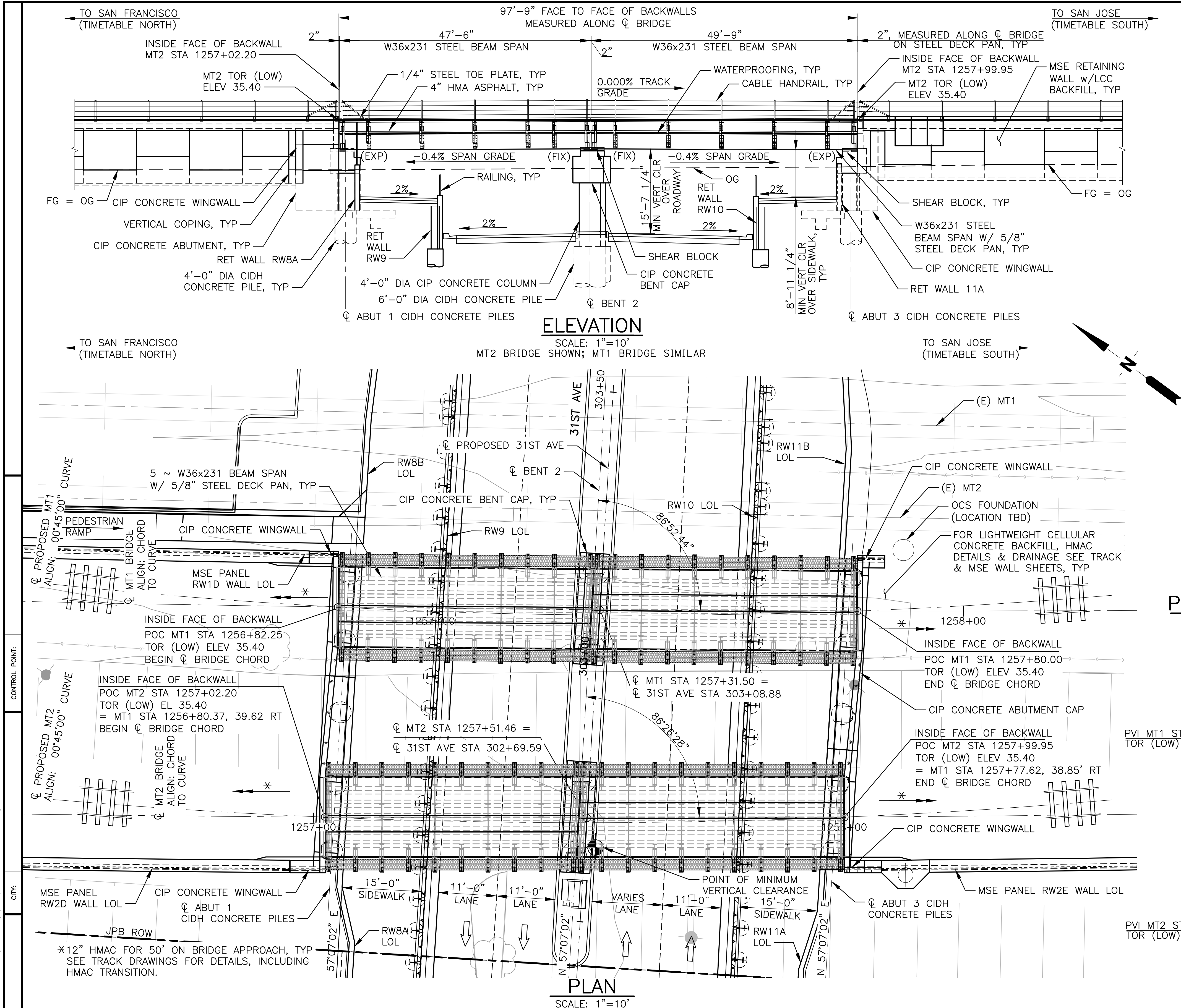
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				SCALE AS SHOWN	
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				DWG NO S302	REV 0







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#### NOTES:

1. THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIALS.
2. FOR GENERAL NOTES AND MATERIAL REQUIREMENTS, SEE DWG NO S001 AND S002.
3. SEE TRACK PLANS FOR TRACK GEOMETRY AND DRAINAGE.
4. SEE CIVIL PLANS FOR ROADWAY GEOMETRY AND DRAINAGE.
5. FOR MSE PANEL WALL RW DETAILS, SEE S500-S528.
6. FOR ABBREVIATIONS AND SYMBOLS, SEE DRAWING B200, SD-1102, SD-1201 AND S002.
7. FOR RW DETAILS, SEE S900-S959.

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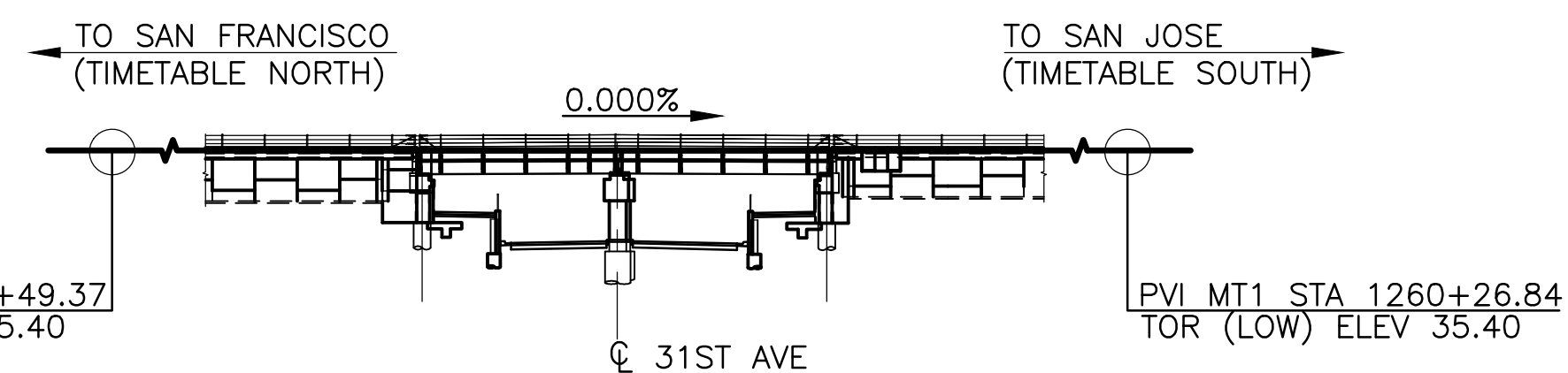
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- COOPER E80 OR ALTERNATE LIVE LOAD WITH IMPACT, MAX OF 30" BALLAST PER AREMA.
- 28-DAY COMPRESSIVE CONCRETE STRENGTH OF 4,000 PSI UON ON S001.
- STRUCTURAL STEEL FOR PRIMARY LOAD CARRYING MEMBERS: ASTM A709 GR 50.
- SEE DWG NO S001 AND S002 FOR COMPLETE INFORMATION.

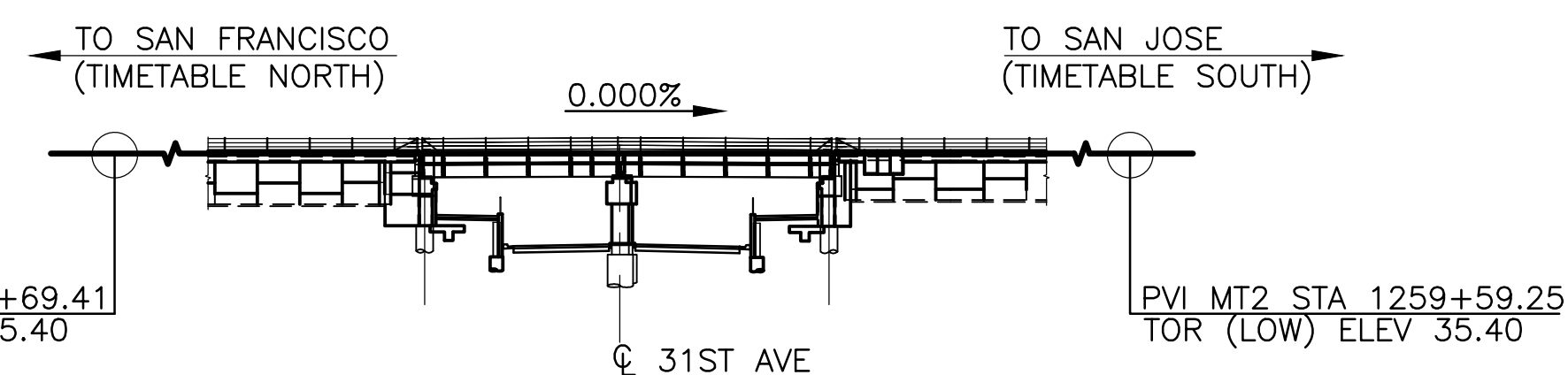
#### LOOKING TIMETABLE NORTH PROPOSED 31st AVE ROAD PROFILE GRADE LINE

NO SCALE  
SEE DWG NO C203 AND C204 FOR DETAILS



#### PROPOSED MT1 PROFILE

LOW RAIL  
NO SCALE



#### PROPOSED MT2 PROFILE

LOW RAIL  
NO SCALE

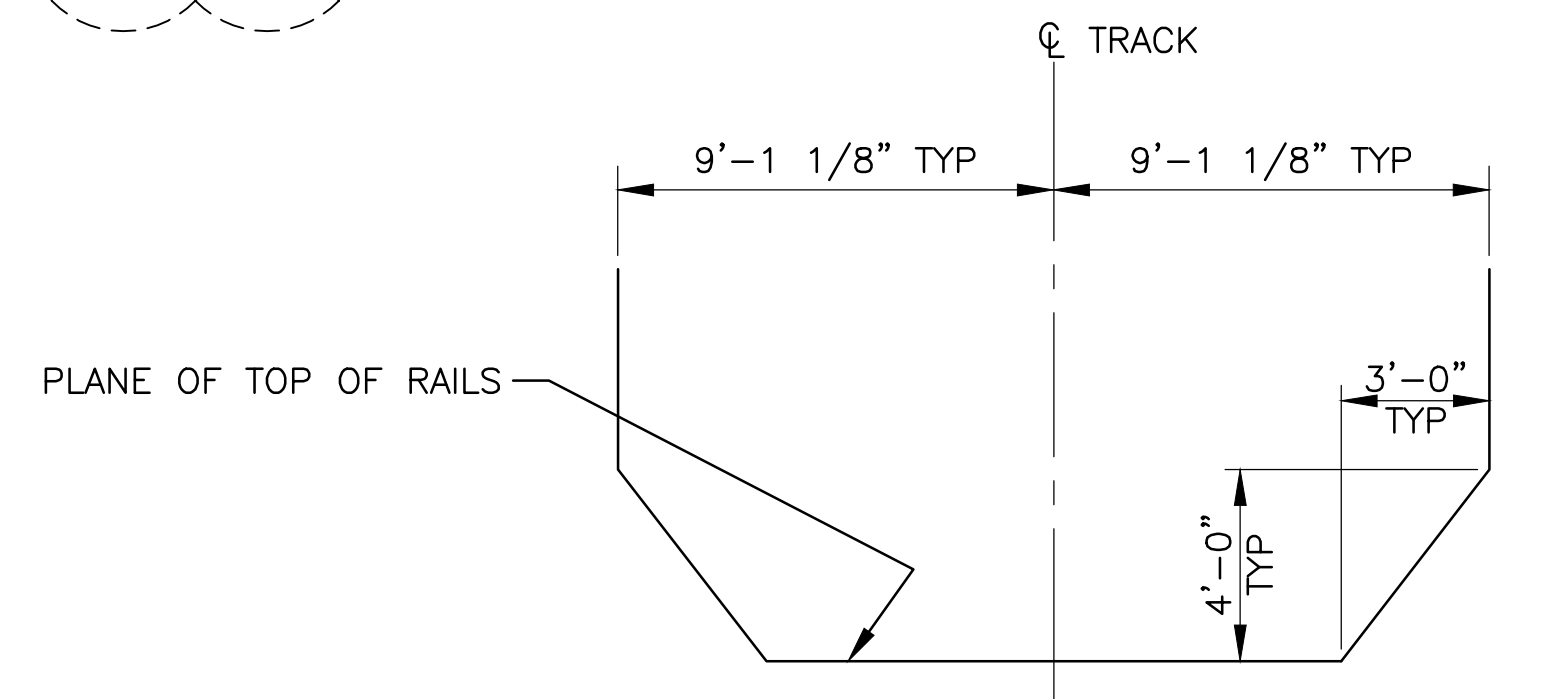
DESIGNED C. BOWERS	 100 Pringle Avenue Suite 400 Walnut Creek, CA 94596 925-974-2500	 1250 San Carlos Avenue San Carlos, CA 94070
DRAWN A. PRITCHARD		
CHECKED P. GRAFF		
IN CHARGE L. RIMBACH		
DATE 12092016		

APPROVED

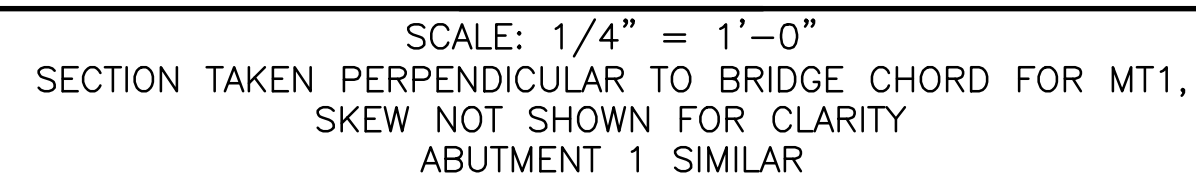
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PROJECT MANAGEMENT R. BOLON		SCALE AS SHOWN		
STRUCTURES C. CHEN		CONTRACT NO 17-J-C-048	MILEPOST 19.0-20.3	
TRACK/CIVIL S. CHAO		DWG NO <b>S400</b>	REV 0	PAGE NO 474
		<b>SAN MATEO 25TH AVENUE GRADE SEPARATION BR 20.15 OVER 31ST AVENUE GENERAL PLAN</b>		

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FOR AREMA CLEARANCE DIAGRAM, SEE DRAWING NO S401.



DISTANCE FROM TOP OF LOW RAIL TO ABUTMENT SEAT	
RAIL + TIE PAD	7 5/8" (-)
CONCRETE TIE AT CENTER OF RAIL	8"
MIN BALLAST	10 3/8"
HMAC	4"
WATERPROOFING	1" (+)
STEEL BEAM + DECK PAN	37 1/8" (3'-1 1/8")
BEARINGS	4"
TOTAL	72 1/8" (6'-0 1/8")

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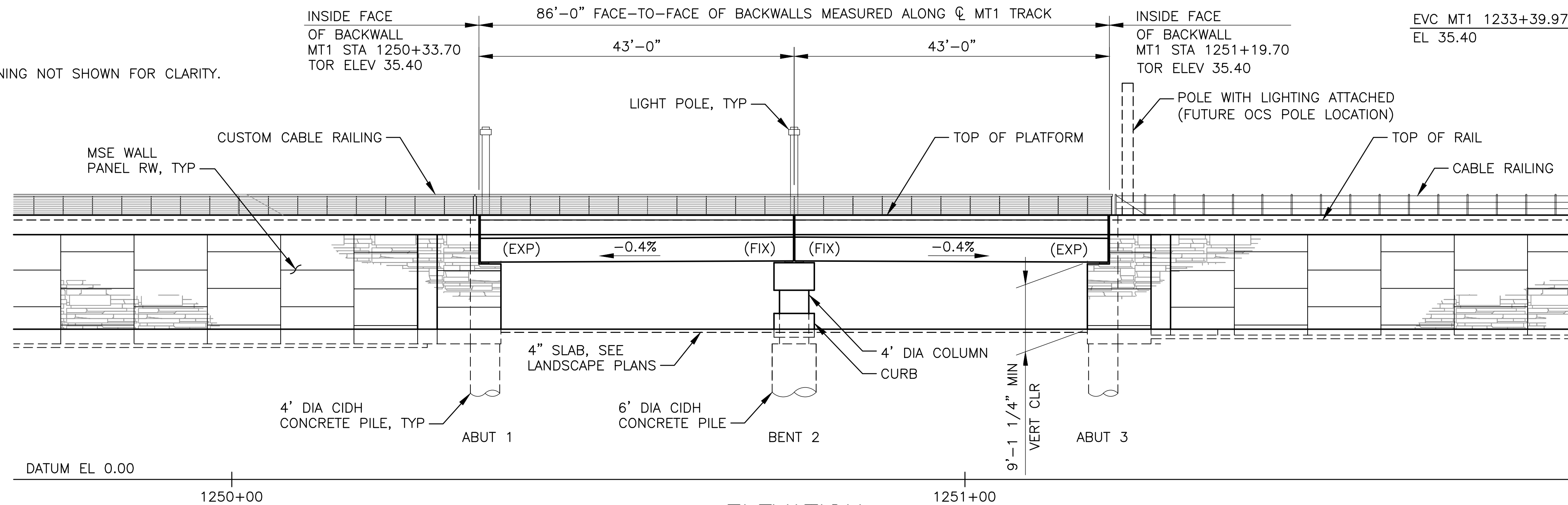
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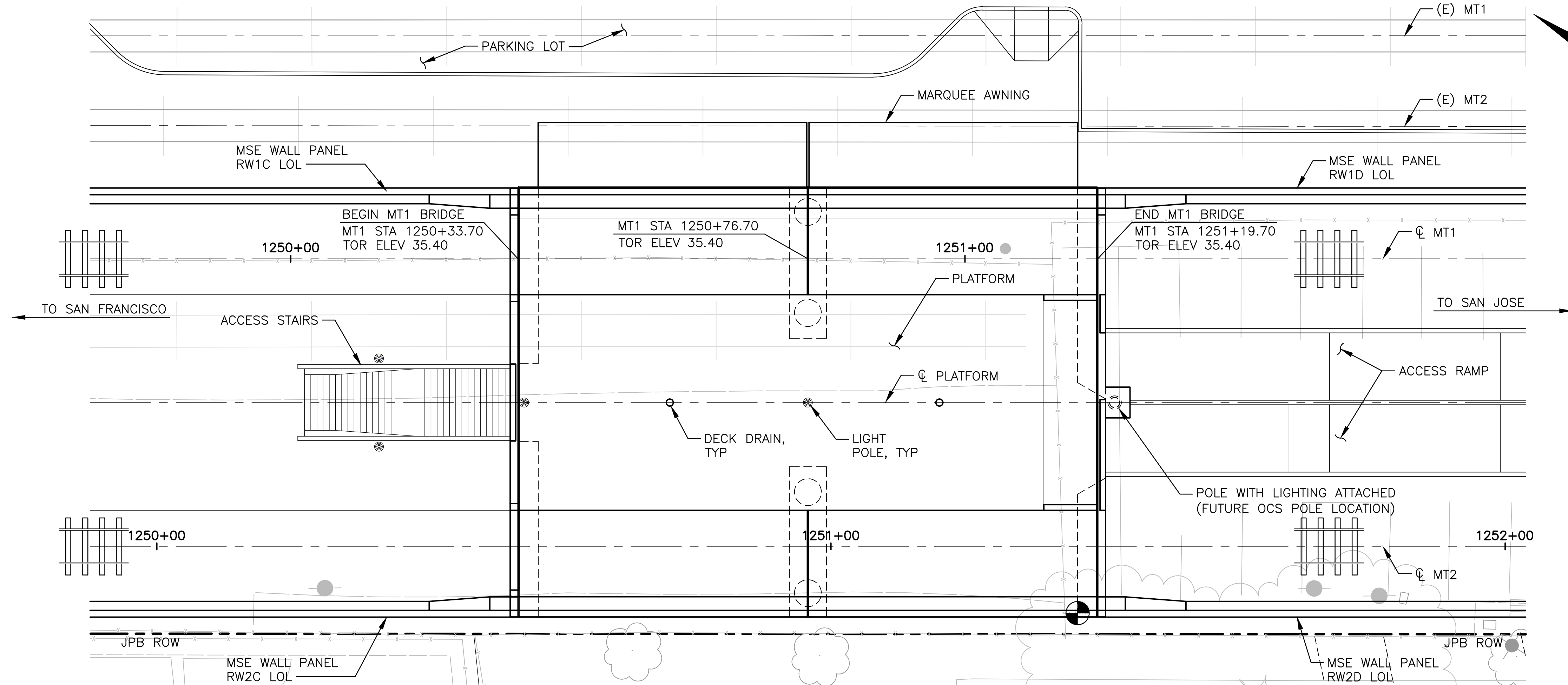
NOTE:  
MARQUEE AWNING NOT SHOWN FOR CLARITY.



**ELEVATION**  
SCALE: 1"=10'

**BRIEF SUMMARY OF DESIGN FOR RECORD:**

- COOPER E80 OR ALTERNATE LIVE LOAD WITH IMPACT, MAX OF 30" BALLAST PER AREMA.
- 28-DAY COMPRESSIVE CONCRETE STRENGTH OF 4,000 PSI, UON ON S001.
- SEE DWG NO S001 AND S002 FOR COMPLETE INFORMATION.



**PLAN**  
SCALE: 1"=10'

**NOTES:**

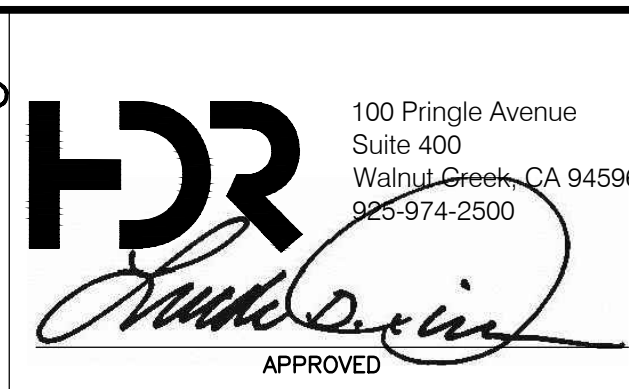
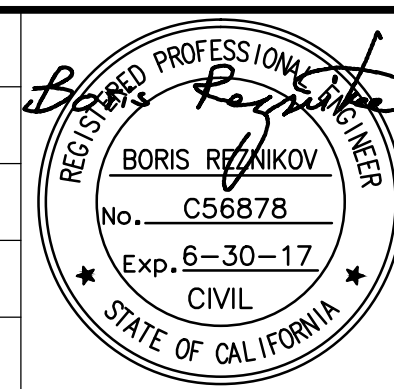
1. THE CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIALS.
2. SEE TRACK PLANS FOR TRACK GEOMETRY.
3. SEE CIVIL PLANS FOR ROADWAY GEOMETRY AND DRAINAGE.
4. FOR ACCESS STAIRS AND RAMP, SEE S540-S565.
5. FOR MSE PANEL WALL RW DETAILS, SEE S500-S528.
6. FOR ABBREVIATIONS AND SYMBOLS, SEE B200, SD-1102, SD-1201 AND S002.
7. FOR CUSTOM CABLE RAILING DETAILS, SEE A530. FOR CABLE RAILING DETAILS, SEE S528.

**LEGEND:**



INDICATES POINT OF MINIMUM VERTICAL CLEARANCE

DESIGNED  
J. SUK  
DRAWN  
J. GREEN  
CHECKED  
T. MEHTA  
IN CHARGE  
L. RIMBACH  
DATE  
12092016



DEPUTY DIRECTOR  
S. CHAO  
PROJECT MANAGEMENT  
R. BOLON  
STRUCTURES  
C. CHEN  
TRACK/CIVIL  
S. CHAO

**PENINSULA CORRIDOR JOINT POWERS BOARD**

**SAN MATEO 25TH AVENUE  
GRADE SEPARATION  
BR 20.03 PEDESTRAIN UNDERPASS  
GENERAL PLAN**

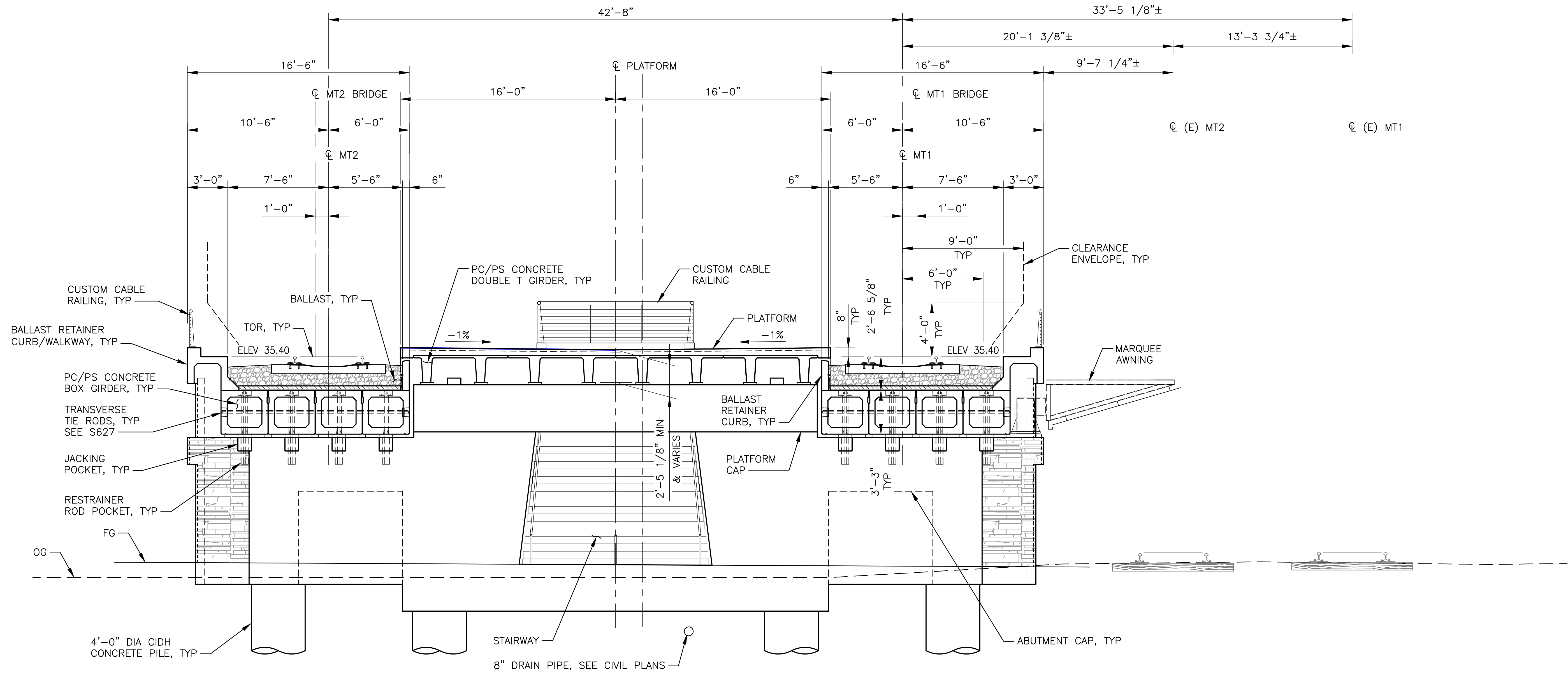
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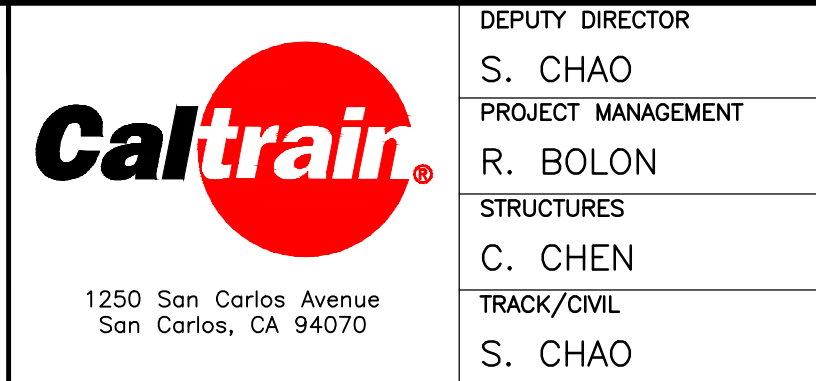
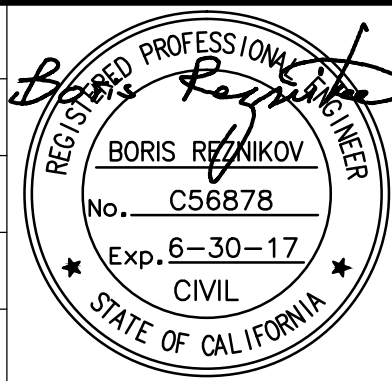


(LOOKING NORTH)  
**TYPICAL SECTION AT ABUTMENT 1**  
SCALE: 1/4" = 1'-0"

DISTANCE FROM TOP OF LOW RAIL TO ABUTMENT SEAT AT CL BRIDGE	
RAIL + TIE PAD	7 5/8" (-)
CONCRETE TIE AT CENTER OF RAIL	8"
MIN BALLAST	10"
HMAC	4"
WATERPROOFING	1" (+)
PC/PS CONC BOX GIRDER	39" (3'-3")
BEARINGS	3"
TOTAL	72 5/8" (6'-0 5/8")

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J. SUK  
DRAWN  
J. GREEN  
CHECKED  
T. MEHTA  
IN CHARGE  
L. RIMBACH  
DATE  
12092016



PENINSULA CORRIDOR JOINT POWERS BOARD		CADD FILE NAME	CADD DATE
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BR 20.03 PEDESTRIAN UNDERPASS		SCALE	
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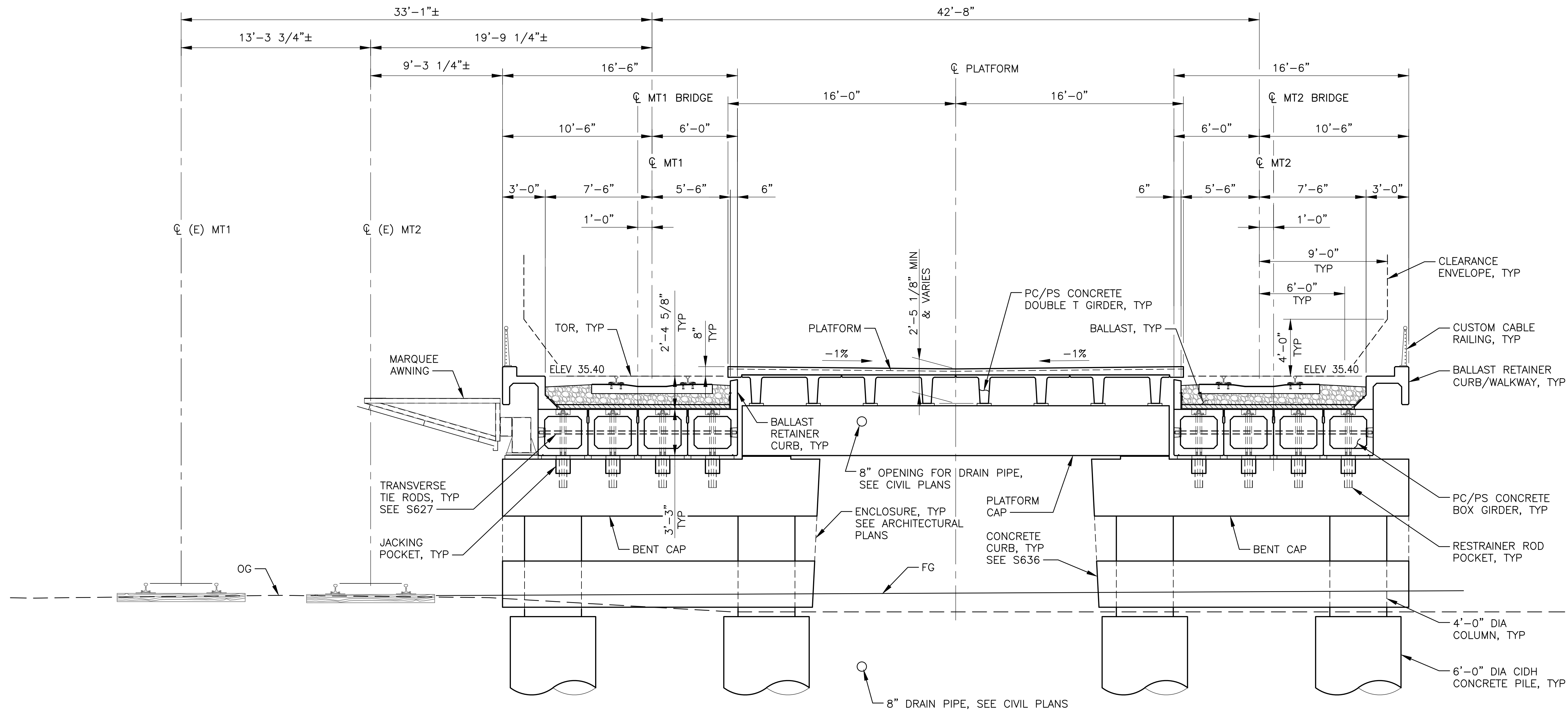


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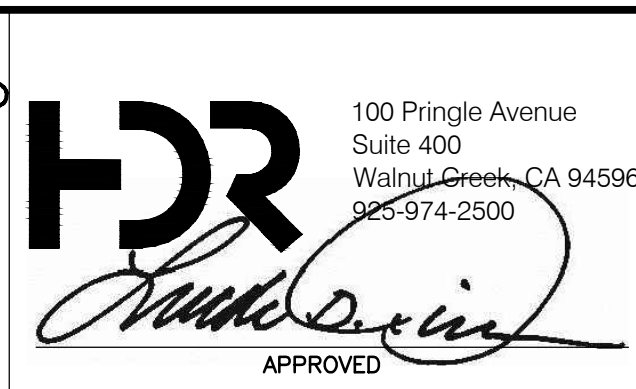
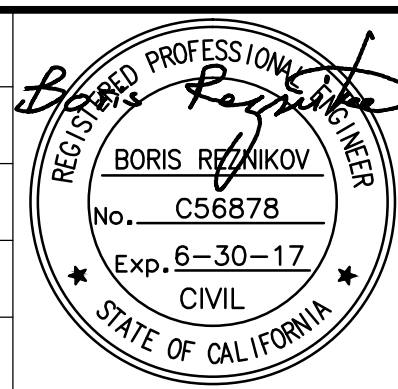


(LOOKING SOUTH)  
**TYPICAL SECTION AT BENT 2**  
SCALE: 1/4" = 1'-0"

DISTANCE FROM TOP OF LOW RAIL TO BENT SEAT AT CL BRIDGE	
RAIL + TIE PAD	7 5/8" (-)
CONCRETE TIE AT CENTER OF RAIL	8"
MIN BALLAST	8"
HMAC	4"
WATERPROOFING	1" (+)
PC/PS CONC BOX GIRDER	39" (3'-3")
BEARINGS	3"
TOTAL	70 5/8" (5'-10 5/8")

REV	DATE	BY	SUB	APP	DESCRIPTION	REV	DATE	BY	SUB	APP	DESCRIPTION
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DESIGNED  
J. SUK  
DRAWN  
J. GREEN  
CHECKED  
T. MEHTA  
IN CHARGE  
L. RIMBACH  
DATE  
12092016



DEPUTY DIRECTOR  
S. CHAO  
PROJECT MANAGEMENT  
R. BOLON  
STRUCTURES  
C. CHEN  
TRACK/CIVIL  
S. CHAO

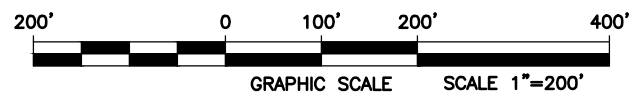
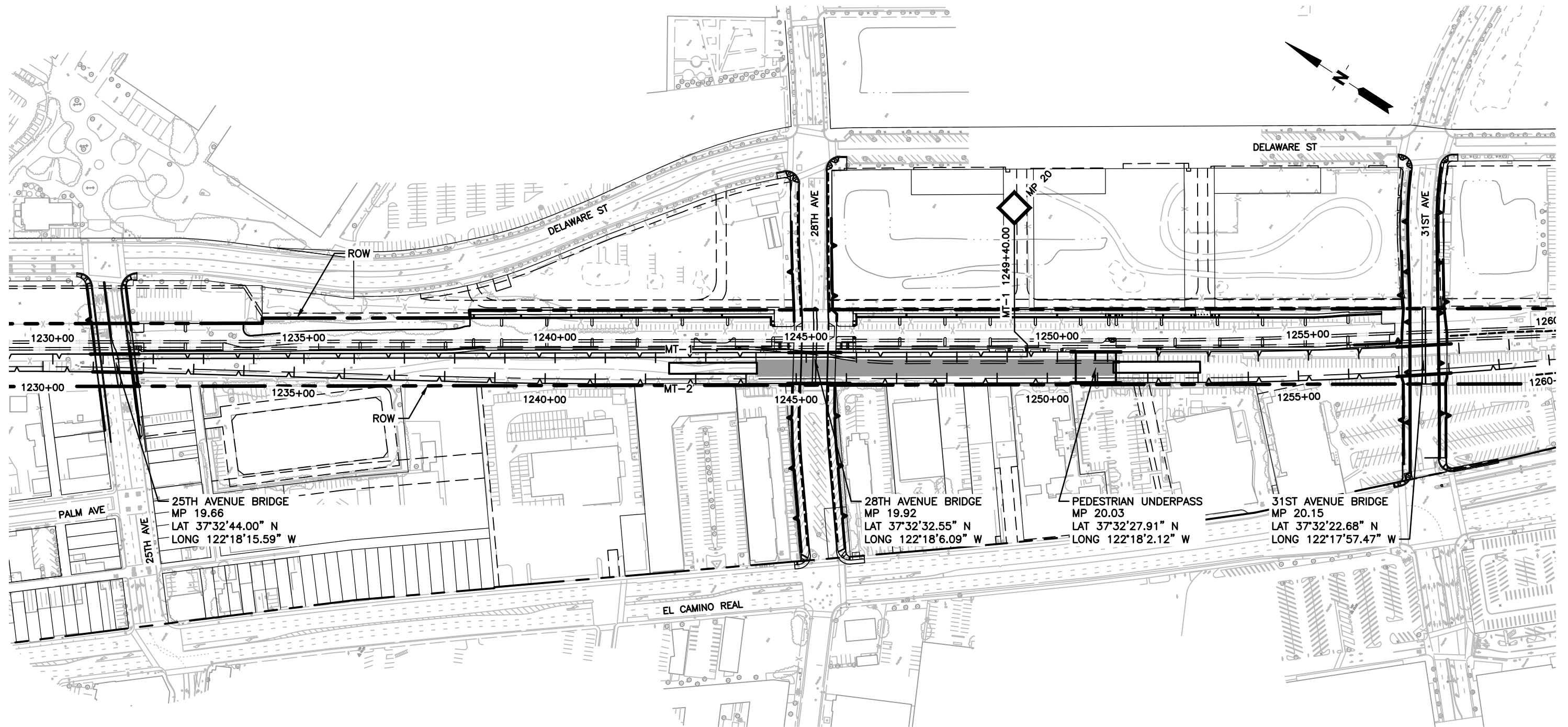
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				SCALE AS SHOWN	
				CONTRACT NO 17-J-C-048	MILEPOST 19.0-20.3
				DWG NO S602	REV 0





## EXHIBIT C

Legal Description



Prepared By:



100 Pringle Ave  
Suite 400  
Walnut Creek, CA 94596  
925-974-2500

EXHIBIT "C"  
CALTRAIN 25TH AVENUE GRADE SEPARATION

TO ACCOMPANY THE PENINSULA CORRIDOR JOINT POWERS BOARD APPLICATION  
TO THE CALIFORNIA PUBLIC UTILITY COMMISSION FOR PERMISSION TO  
CONSTRUCT NEW PEDESTRIAN AND VEHICULAR UNDERPASSES

April 14, 2017

## EXHIBIT D

Memo regarding CEQA exemption



## Memorandum

**To:** San Mateo 25<sup>th</sup> Avenue Grade Separation- Project CEQA File  
**From:** Hilda Lafebre, Capital Projects & Environmental Planning   
**cc:** Leo Tidd, The Louis Berger Group - Environmental Consultant  
**Date:** March 14, 2016  
**Re:** San Mateo 25<sup>th</sup> Avenue Grade Separation Project CEQA Exemption

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The purpose of this memorandum is to document the CEQA statutory exemptions for the San Mateo 25<sup>th</sup> Avenue Grade Separation Project.

### Project Description

The San Mateo 25<sup>th</sup> Avenue Grade Separation Project (the proposed project) is located along the Caltrain corridor between the existing Caltrain undercrossing of State Highway 92 to the north and the existing Caltrain Bridge over Hillsdale Boulevard to the south, a distance of approximately 1.2 miles. The purpose of the proposed project is to: (1) improve public safety by eliminating an existing at-grade railroad crossing at 25<sup>th</sup> Avenue, (2) support transit-oriented development planned in this area of the City of San Mateo, and (3) upgrade the Hillsdale Station to prepare it for growing demand from new development in the area and address pedestrian access deficiencies of the existing station configuration.

This proposed project is intended to be built prior to the electrification of the Caltrain corridor, currently expected to be completed in 2020. Therefore, the proposed project has been designed to be compatible with future electrified service and infrastructure, but it does not include the actual construction of such infrastructure.

The proposed project consists of the reconstruction, realignment, and elevation of tracks within the project limits on embankment, retaining-wall and bridge viaduct to accomplish grade separation of the existing at-grade rail crossing at 25<sup>th</sup> Avenue, and the creation of new grade-separated crossings over extended 28<sup>th</sup> and 31<sup>st</sup> avenues, and over a new pedestrian undercrossing at 29½ Avenue, none of which presently have through-passage across the Caltrain tracks.

The proposed project also includes the relocation and reconfiguration of Caltrain's Hillsdale Station platform from south of 31<sup>st</sup> Avenue to north of 31<sup>st</sup> Avenue to 1) allow the grade-separated and re-aligned tracks to tie into the existing tracks at the existing Caltrain bridge over Hillsdale Boulevard, 2) eliminate the existing at-grade pedestrian crossing of active tracks associated with the existing two side-platform station configuration by changing it to a center platform configuration, and 3) allow pedestrian access, including ADA-compliant access, to the

### PENINSULA CORRIDOR JOINT POWERS BOARD

1250 San Carlos Ave. – P.O. Box 3006  
San Carlos, CA 94070-1306 (650) 508-6219



newly constructed platform from street level at 28<sup>th</sup> Avenue and the new pedestrian way at 29½ Avenue.

With the implementation of the proposed project, there will be a net expansion of parking at the Hillsdale Station from 518 parking spaces to 638 parking spaces. The track realignment will remove 178 existing parking spaces on the west side of the Caltrain corridor at the station. Two new parking lots totaling 298 parking spaces will be built adjacent to the east side of the realigned tracks and within the right-of-way now occupied by tracks that will be removed for the project. The southern lot will be situated between 31<sup>st</sup> and 28<sup>th</sup> avenues, and the northern lot will extend north of 28<sup>th</sup> Avenue to the approximate midpoint between 28<sup>th</sup> and 25<sup>th</sup> avenues.

The project is undergoing an environmental review under the National Environmental Policy (NEPA), with FTA as the lead federal agency.

### **Statutory Exemptions from CEQA**

The primary component of and driver for the proposed project, replacing the existing grade crossing at 25<sup>th</sup> Ave. with a grade separation, is exempt from CEQA pursuant to Section 21080.13 of the Public Resources Code (CEQA Guidelines Article 18 § 15282), which provides exemption for any railroad grade separation project which eliminates an existing grade crossing or which reconstructs an existing grade separation. The grade separation of 25<sup>th</sup> Avenue necessitates shifting the railroad alignment to the west of the existing tracks in order to maintain service during construction. The westerly alignment shift in turn requires relocating the station because the existing station platform would conflict with the shift in the track. As integral elements of the 25<sup>th</sup> Avenue grade separation, the alignment shift and station relocation are exempt from CEQA.

The remaining project components (including the extension of 28<sup>th</sup> and 31<sup>st</sup> Avenue and station parking improvements) are covered by a broader statutory exemption for projects that increase passenger service on existing rail lines, including transit station and parking modernization (CEQA Guidelines Article 18 § 15275). The proposed project does not increase frequency of passenger service, but is supportive of independent initiatives that do involve increased service, including the Caltrain Electrification program. The Hillsdale Station relocation and upgrades (such as the central platform) would support increased ridership, as would the proximity of the new station location (between 28<sup>th</sup> and 31<sup>st</sup> Ave.) to the center of the Bay Meadows transit-oriented development. The scope of station improvements to the Hillsdale Station is consistent with the concept of “modernization.” The grade separations of 28<sup>th</sup> and 31<sup>st</sup> Ave. are essential to the station modernization by accommodating bicycle/pedestrian movements under the tracks for station access. The new grade separations and extension of 28<sup>th</sup> and 31<sup>st</sup> Ave. are also part of the station access needs for efficient auto and bus traffic flow in the general area.

### **Conclusion**

The proposed project is exempt from CEQA based on the statutory exemption for grade separations (CEQA Guidelines Article 18 § 15282) and the statutory exemption for projects that support increased transit service/ridership (CEQA Guidelines Article 18 § 15275). Therefore, no further review under CEQA is required.

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## PUBLIC UTILITIES COMMISSION

505 VAN NESS AVENUE

SAN FRANCISCO, CA 94102-3298



Dear Rail Crossing Applicant:

Subject: SB 960 Supplement to Application for Railroad Crossings

Applications filed after January 1, 1998, are required to include the following information:

1. Category
2. Need for hearing
3. Issues
4. Schedule

A well-prepared application should contain a clear and concise summary of the issues that the Commission is being asked to resolve. If your application already contains a summary of the issues to be considered by the Commission, simply indicate on the form the section or page of your application where the issues are summarized. If your application does not contain this information, please summarize the issues on the attached form or on a separate page.

Attachments



## **Scoping Memo Information for Applications**

### **A. Category (Check the category that is most appropriate)**

☐ **Adjudicatory** - “Adjudicatory” proceedings are: (1) enforcement investigations into possible violations of any provision of statutory law or order or rule of the Commission; and (2) complaints against regulated entities, including those complaints that challenge the accuracy of a bill, but excluding those complaints that challenge the reasonableness of rates or charges, past, present, or future, such as **formal rough crossing complaints** (maximum 12 month process if hearings are required).

☒ **Ratesetting** - “Ratesetting” proceedings are proceedings in which the Commission sets or investigates rates for a specifically named utility (or utilities), or establishes a mechanism that in turn sets the rates for a specifically named utility (or utilities). “Ratesetting” proceedings include complaints that challenge the reasonableness of rates or charges, past, present, or future. Other proceedings may also be categorized as ratesetting when they do not clearly fit into one category, such as **railroad crossing applications** (maximum 18 month process if hearings are required).

☐ **Quasi-legislative** - “Quasi-legislative” proceedings are proceedings that establish policy or rules (including generic ratemaking policy or rules) affecting a class of regulated entities, including those proceedings in which the Commission investigates rates or practices for an entire regulated industry or class of entities within the industry.

**B. Are hearings necessary?** ☐ **Yes** ☒ **No**

If yes, identify the material disputed factual issues on which hearings should be held, and the general nature of the evidence to be introduced. Railroad crossing applications which are not controversial usually do not require hearings.

Are public witness hearings necessary?

☐ **Yes** ☒ **No**

Public witness hearings are set up for the purpose of getting input from the general public and any entity that will not be a party to the proceeding. Such input usually involves presenting written or oral statements to the presiding officer, not sworn testimony. Public witness statements are not subject to cross-examination.

**C. Issues** - List here the specific issues that need to be addressed in the proceeding.

**All issues have been worked out before the filing of this application. No objections or issues need to be worked out that the applicant is aware of.**

**D. Schedule (Even if you checked “No” in B above)** Should the Commission decide to hold hearings, indicate here the proposed schedule for completing the proceeding within 12 months (if categorized as adjudicatory) or 18 months (if categorized as ratesetting or quasi-legislative).

The schedule should include proposed dates for the following events as needed:

- 6 months Prehearing conference
- 9 months Hearings
- 12 months Briefs due
- 13 months Submission
- 15 months Proposed decision (90 days after submission)
- 18 months Final decision (60 days after proposed decision is mailed)