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**BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA**

A1708019

Application of the California High-Speed Rail Authority to construct proposed high-speed tracks and underpass grade separations at SR99 (MP 195.74) and SR99 Ramp (MP 195.83) within the City of Fresno, California.

Application No. \_\_\_\_\_

**APPLICATION**

The California High-Speed Rail Authority (CHSRA or Applicant) hereby respectfully requests authority from the California Public Utilities Commission (CPUC or Commission) to construct proposed tracks and highway-rail underpass grade separations in the City of Fresno at State Route 99 (SR99), and the southbound entrance ramp to State Route 99 from S. Cedar Avenue (SR99 Ramp), which are part of the proposed California High-Speed Train System (CHSTS).

In support of this application the Applicant asserts that:

1. The Applicant's exact legal name is California High-Speed Rail Authority and CHSRA is a public agency formed pursuant to the California High-Speed Rail Act (California Public Utilities Code § 185000 *et seq.*).
2. The Applicant's principal place of business is California High-Speed Rail Authority, 770 L Street, Suite 620; Sacramento, CA 95814. The main telephone number is (916) 324-1541.
3. All correspondence, communication, notices, orders and other papers relative to this application should be addressed to: Bruce W. Armistead, Director of

Operations and Maintenance, at the above address and at the following e-mail address: [Bruce.Armistead@hsr.ca.gov](mailto:Bruce.Armistead@hsr.ca.gov). Mr. Armistead is an employee of CHSRA and the Applicant's authorized agent.

4. The proposed mainline tracks and underpass grade separations at SR99, and SR99 Ramp are a part of the California High-Speed Train Project Construction Package 1 (CP1 Project), which is the first construction package of the CHSTS. CHSRA is working cooperatively with the stakeholders along the corridor, which include government agencies and railroads.

The CP1 Project limits begin in Madera County in the north and extend south through the City of Fresno, into unincorporated Fresno County. The CP1 Project length is approximately thirty miles and consists of two CHSTS mainline tracks, generally aligned in a north-south direction.

Grade separations allow for the safe and simultaneous movement of highway and rail modes of travel. With that consideration, the proposed highway-rail and rail-rail crossings of the CP1 Project include nineteen (19) new grade separations, modification of thirteen (13) existing at-grade crossings to grade separations, and modification of six (6) existing grade separations. Eight (8) existing public at-grade crossings and one (1) grade separated crossing are proposed to be closed. No new at-grade crossings are proposed for the CP1 Project.

For the protection of the general public and CHSTS passengers, the CHSTS will be a closed corridor. Unauthorized access to CHSRA right-of-way will be protected with the use of a continuous Access Restricted (AR) fence or barrier. The typical AR fence consists of an eight foot (8 ft) welded wire mesh, which is more resistant to climbing and forced entry than chain link fence. When required by CHSRA criteria, expanded metal mesh fence or barrier walls will be used. In addition to the AR fence or barrier walls, intrusion monitoring and detection are proposed. CHSRA will employ security services to enforce right-of-way access restrictions.

The overhead contact system, which will supply energy to the rail vehicles, is consistent with the requirements of General Orders 26-D, 95, 128 and 176.

5. This application and relief sought herein is filed pursuant to Sections 1201 through 1205, inclusive, of the Public Utilities Code and in accordance with Rules 3.7 and 3.9 of the Commission's Rules of Practice and Procedure.
6. The CHSRA, in cooperation with the City of Fresno and the California Department of Transportation District 6 (Caltrans), proposes to construct two CHSTS mainline tracks and underpass grade separation structures at SR99, and SR99 Ramp in the City of Fresno. The existing roadways will remain at-grade, and the proposed mainline tracks will be constructed on elevated structures.

For each of the proposed crossings, the CHSTS mile post, proposed CPUC crossing number and DOT crossing number are listed in the following table. These crossings are along the CHSTS Sierra Subdivision.

<b>Crossing Name</b>	<b>Mile Post</b>	<b>Proposed CPUC #</b>	<b>Proposed DOT #</b>
State Route 99	195.74	135S-195.74-B	968 530R
State Route 99 Ramp	195.83	135S-195.83-B	968 531X

7. The legal descriptions for the locations of the proposed CHSTS grade separations at SR99, and SR99 Ramp (Rule 3.7(a)) are provided in Exhibit A, attached hereto and made a part hereof.
8. The crossing identification numbers of the nearest existing public crossings on each side of the proposed crossings (Rule 3.7(b)) are as follows: the nearest existing crossing to the north of the proposed crossings at SR99 and SR99 Ramp is the BNSF spur crossing at S. Cedar Avenue (CPUC No. 002-994.6-C and DOT No. 028466A) and to the south is the BNSF crossing at S. Calway OH SR99 (CPUC No. 002-993.8-A and DOT No. 028463E). These crossings are located in the City of Fresno.
9. CP1 Project design drawings showing the general vicinity (Rule 3.7 (e)), proposed horizontal and vertical alignment for the two CHSTS tracks (Rule 3.7 (f) and 3.9 (c)), and structure design drawings (Rule 3.7 (d)) are provided in Exhibit B, attached hereto and made a part hereof. Since the roadways over which the rail lines will be constructed are remaining in place with minimal or no modification, roadway plan and profile drawings for SR99, and SR99 Ramp (Rule

3.7 (d) and Rule 3.7 (f)) are not included. The width of existing roadways and vertical clearance dimensions are provided on the structural drawings in Exhibit B.

The following tables summarize the minimum temporary and permanent clearances illustrated on the design drawings in Exhibit B. The permanent clearances meet or exceed those required by General Order 26-D.

<b>Temporary Clearances</b>	<b>Roadway Clearances</b>		<b>Track Clearances</b>	
	<b>Minimum Vertical</b>	<b>Minimum Horizontal</b>	<b>Minimum Vertical</b>	<b>Minimum Horizontal</b>
State Route 99	15' 4"	40' 0" (total width)	N/A	N/A
State Route 99 Ramp	15' 4"	20' 0" (total width)	N/A	N/A

<b>Permanent Clearances</b>	<b>Roadway Clearances</b>		<b>Track Clearances</b>	
	<b>Minimum Vertical</b>	<b>Minimum Horizontal</b>	<b>Minimum Vertical</b>	<b>Minimum Horizontal</b>
State Route 99	33' 9"	56' (NB total width)	N/A	>10' *
State Route 99 Ramp	19' 8"	24' (total width)	N/A	>10' *
* Distance is from centerline of track to face of OCS pole.				

10. In support of the proposed underpass grade separations at SR99, and SR99 Ramp, a letter of concurrence from Caltrans is provided in Exhibit C, attached hereto and made a part hereof.

The agreement between CHSRA and Caltrans (Rule 3.9 (a)), entitled *Master Agreement between the California High-Speed Rail Authority and the California Department of Transportation for Construction Project Work Located within the CROW [Caltrans Right of Way] (Post PA&ED)*, was provided to the Commission as Exhibit C2 to Application A.16-05-016, and is made a part hereof by reference. A Notice of Availability and Certificate of Service for the CHSRA and Caltrans agreement are filed with this application.

11. The CHSRA, as the lead agency under the provisions of the California Environmental Quality Act (CEQA), prepared the *Final Environmental Impact*

*Report/ Environmental Impact Statement (Final EIR/EIS) for the Fresno to Bakersfield Section* of the California High-Speed Train Project. The Federal Railroad Administration (FRA) was the lead agency under the National Environmental Policy Act (NEPA). The Notice of Determination (NOD) for the *Final EIR/EIS* was certified on May 7, 2014 (State Clearinghouse No. 2009091126), and the Record of Decision (ROD) was dated June 27, 2014. The *Final EIR/EIS for the Fresno to Bakersfield Section* of the California High-Speed Train Project was provided to the Commission (Rule 2.4) on archival and standard CD-ROM/DVD as Exhibit D to Application A. 17-05-014. The Final EIR/EIS is made a part hereof by reference. A Notice of Availability for the *Final EIR/EIS* and Certificate of Service are filed with this application.

12. Attached hereto as Exhibit D is the Scoping Memo for this application.
13. Attached hereto as Exhibit E is the Verification for this application.

WHEREFORE, the Applicant respectfully requests:

1. That the Public Utilities Commission of the State of California issue an Order authorizing construction of the underpass grade separations of SR99, and SR99 Ramp and CHSTS mainline tracks pursuant to the provisions of Sections 1201-1205, inclusive, of California Public Utilities Code.
2. That the Order provides seventy-two (72) months from the date of such an Order within which to complete the work requested. This extended period of time is requested due to the complexity, size and unique nature of the CP1 Project.

The CP1 Project scope includes the design and construction of the structures and track roadbed, encompassing nearly 50 crossings. To complete construction at the subject crossings and all of the crossings along the length of the CP1 Project, CHSRA anticipates issuing a separate contract within several years for construction of the proposed tracks and overhead contact system. The construction of the CP1 Project and the future track work contract will not be complete within the standard 36-month authorization period but is likely to be completed within 72 months. In addition, since the proposed crossings are grade separations, the clearances and crossing configurations are well defined and not

subject to the variability common for at-grade crossings. Given the number of new CHSTS crossing requests that will be submitted to the CPUC in the coming months and years, authorizing the extended period for this crossing will relieve both the CP1 Project and CPUC staff of preparing and processing numerous crossing extensions that will result with a 36-month Order.

Signed

A handwritten signature in black ink, appearing to read 'B. Armistead', is written over a horizontal line.

Bruce W. Armistead  
Director of Operations and Maintenance  
California High-Speed Rail Authority  
770 L Street, Suite 620  
Sacramento, CA 95814  
Tel: (916) 324-1541  
E-Mail: [Bruce.Armistead@hsr.ca.gov](mailto:Bruce.Armistead@hsr.ca.gov)

# List of Exhibits

<b>Exhibit A</b>	<b>Crossing Legal Descriptions</b>	
	A1 – State Route 99	
	A2 – State Route 99 Ramp	
<b>Exhibit B</b>	<b>Project Drawings</b>	
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	B2 – Structure Drawings	Page 2
	General Plans & Elevations	Page 2
	Cross Sections	Page 5
	B3 – Guideway Drawings	Page 8
	Track Chart	Page 8
	Guideway Plan and Profile	Page 9
<b>Exhibit C</b>	<b>Local Project Support</b>	
	Caltrans Concurrence	
<b>Exhibit D</b>	<b>Scoping Memo</b>	
<b>Exhibit E</b>	<b>Verification</b>	

## Supporting Documents

### Notice of Availability

Exhibit C2 to Application A. 16-05-016

*Master Agreement between the California High-Speed Rail Authority and  
the California Department of Transportation for  
Construction Project Work within the CROW (Post PA&ED)*

&

Exhibit D to Application A. 17-05-014

*Final Environmental Impact Report/ Environmental Impact Statement for  
the Fresno to Bakersfield Section of the  
California High-Speed Train Project*

### Certificate of Service

# **Exhibit A**

## **Crossing Legal Descriptions**

### **Exhibit A1 – State Route 99**

EXHIBIT "A"  
LEGAL DESCRIPTION

PARCEL FB-10-9098 (AERIAL EASEMENT at State Route 99)

An aerial easement over and across that portion of land situated in the City of Fresno, County of Fresno, State of California, being a portion of the Northwest Quarter of Section 25 all in Township 14 South, Range 20 East, Mount Diablo Base and Meridian, said portion described as follows;

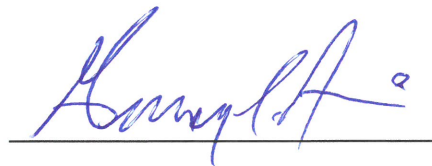
**COMMENCING** at the Northwest Quarter Corner of said Section 25; thence along the West Line of said Section 25, South  $00^{\circ}34'59''$  West, 917.40 feet to the beginning of a non-tangent curve concave southwesterly, to which point a radial line bears North  $71^{\circ}07'08''$  East; thence leaving said West Line, southeasterly along said curve, having a radius of 21318.25 feet, through a central angle of  $00^{\circ}45'35''$ , and arc length of 282.63 feet to the northerly outside edge of bridge deck and the **POINT OF BEGINNING**,

Thence northeasterly leaving said curve, along the outside edge of said deck, North  $72^{\circ}28'06''$  East, 4.65 feet to the northeasterly corner of said deck; thence southeasterly along the outside edge of said deck the following seventeen (17) courses and distances: (1) South  $17^{\circ}31'54''$  East, 20.00 feet; (2) thence South  $06^{\circ}13'18''$  East, 5.10 feet; (3) thence South  $17^{\circ}31'54''$  East, 56.83 feet; (4) thence South  $28^{\circ}50'30''$  East, 5.10 feet; (5) thence South  $17^{\circ}31'54''$  East, 0.67 feet to the beginning of a non-tangent curve concave southwesterly, to which point a radial line bears North  $09^{\circ}44'53''$  East; (6) thence southeasterly along said curve, having a radius of 6.00 feet, through a central angle of  $125^{\circ}26'27''$ , and arc length of 13.14 feet; (7) thence leaving said curve, South  $17^{\circ}31'54''$  East, 14.67 feet; (8) thence South  $06^{\circ}13'18''$  East, 5.10 feet; (9) thence South  $17^{\circ}31'54''$  East, 122.00 feet; (10) thence South  $28^{\circ}50'30''$  East, 5.10 feet; (11) thence North  $81^{\circ}12'52''$  East, 3.29 feet; (12) thence South  $17^{\circ}31'54''$  East, 39.00 feet; (13) thence South  $63^{\circ}43'20''$  West, 3.29 feet; (14) thence South  $06^{\circ}13'18''$  East, 5.10 feet; (15) thence South  $17^{\circ}31'54''$  East, 122.00 feet; (16) thence South  $28^{\circ}50'30''$  East, 5.10 feet; (17) thence South  $17^{\circ}31'54''$  East, 20.00 feet the southeasterly corner of said deck; thence southwesterly along the outside edge of said deck, South  $72^{\circ}28'06''$  West, 51.17 feet to the southwesterly corner of said deck; thence northwesterly along the outside edge of said deck the following seventeen (17) courses and distances: (1) thence North  $17^{\circ}31'54''$  West, 20.00 feet; (2) thence North  $06^{\circ}13'18''$  West, 5.10 feet; (3) thence North  $17^{\circ}31'54''$  West, 56.83 feet; (4) thence North  $28^{\circ}50'30''$  West, 5.10 feet; (5) North  $17^{\circ}31'54''$  West, 0.67 feet to the beginning of a non-tangent curve concave northeasterly, to which point a radial line bears South  $09^{\circ}44'53''$  West; (6) thence northwesterly along said curve, having a radius of 6.00 feet, through a central angle of  $125^{\circ}26'27''$ , and arc length of 13.14 feet; (7) thence North  $17^{\circ}31'54''$  West, 14.67 feet; (8) thence North  $06^{\circ}13'18''$  West, 5.10 feet; (9) thence North  $17^{\circ}31'54''$  West, 122.00 feet; (10) thence North  $28^{\circ}50'30''$  West, 5.10 feet; (11) thence South  $81^{\circ}12'52''$  West, 3.29 feet; (12) thence North  $17^{\circ}31'54''$  West, 39.00 feet; (13) thence North  $63^{\circ}43'20''$  East, 3.29 feet; (14) thence North  $06^{\circ}13'18''$  West, 5.10 feet; (15) thence North  $17^{\circ}31'54''$  West, 122.00 feet; (16) thence North  $28^{\circ}50'30''$  West, 5.10 feet; (17) thence North  $17^{\circ}31'54''$  West, 20.00 feet to the northwesterly corner of said deck; thence northeasterly along the outside edge of said deck, North  $72^{\circ}28'06''$  East, 46.52 feet to the **POINT OF BEGINNING**.

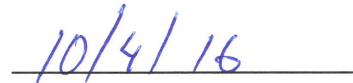
The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000066514 to obtain ground level distances.

END OF DESCRIPTION

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.



Gregory L. Rice  
Licensed Land Surveyor  
California No. 8201



Date

# **Exhibit A**

## **Crossing Legal Descriptions**

### **Exhibit A2 – State Route 99 Ramp**

EXHIBIT "A"  
LEGAL DESCRIPTION

PARCEL FB-10-9099 (AERIAL EASEMENT at State Route 99/Cedar Avenue SB On-Ramp)

An aerial easement over and across that portion of land situated in the City of Fresno, County of Fresno, State of California, being a portion of the Southwest Quarter of the Northwest Quarter of Section 25, Township 14 South, Range 20 East, Mount Diablo Base and Meridian, said portion described as follows;

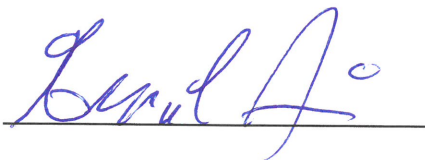
**COMMENCING** at the most northeasterly corner of that certain parcel as described in Book 7833 at Page 786, recorded May 18, 1978, Official Records of said County, said point also being on the southwesterly line of U.S. Highway 99 as described in said Book 7833 at Page 786; thence along said southwesterly line, North 68°51'41" West, 22.46 feet to the southerly outside edge of bridge deck and the **POINT OF BEGINNING**

Thence southwesterly along the outside edge of said deck, South 73°59'59" West, 19.43 feet to the southwesterly corner of said deck, said point being the beginning of a non-tangent curve concave southwesterly, to which point a radial line bears North 73°59'59" East; thence northwesterly along said curve, and along the outside of edge of said deck, having a radius of 21275.25 feet, through a central angle of 00°29'04", and arc length of 179.89 feet to the northwesterly corner of said deck; thence leaving said curve, along the outside edge of said deck, North 73°30'55" East, 43.00 feet to the northeasterly corner of said deck, said point being the beginning of a non-tangent curve concave southwesterly, to which point a radial line bears North 73°30'55" East; thence southeasterly along said curve, and along the outside of edge of said deck, having a radius of 21318.25 feet, through a central angle of 00°29'04", and arc length of 180.25 feet to the southeasterly corner of said deck; thence leaving said curve, along the outside edge of said deck, South 73°59'59" West, 23.57 feet to the **POINT OF BEGINNING**.

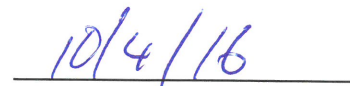
The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000066514 to obtain ground level distances.

END OF DESCRIPTION

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.



Gregory L. Rice  
Licensed Land Surveyor  
California No. 8201



Date

# **Exhibit B**

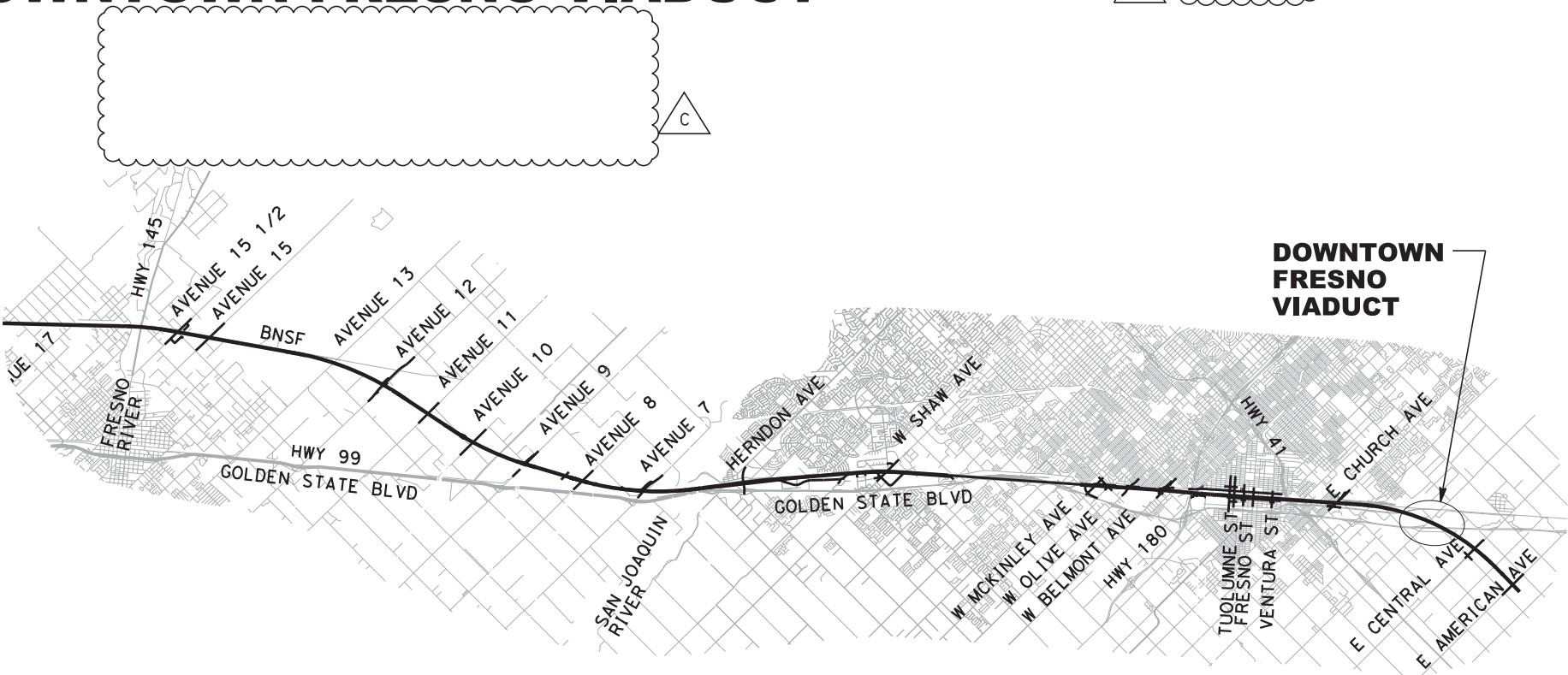
## Project Drawings

### **Exhibit B1 – Vicinity Map**

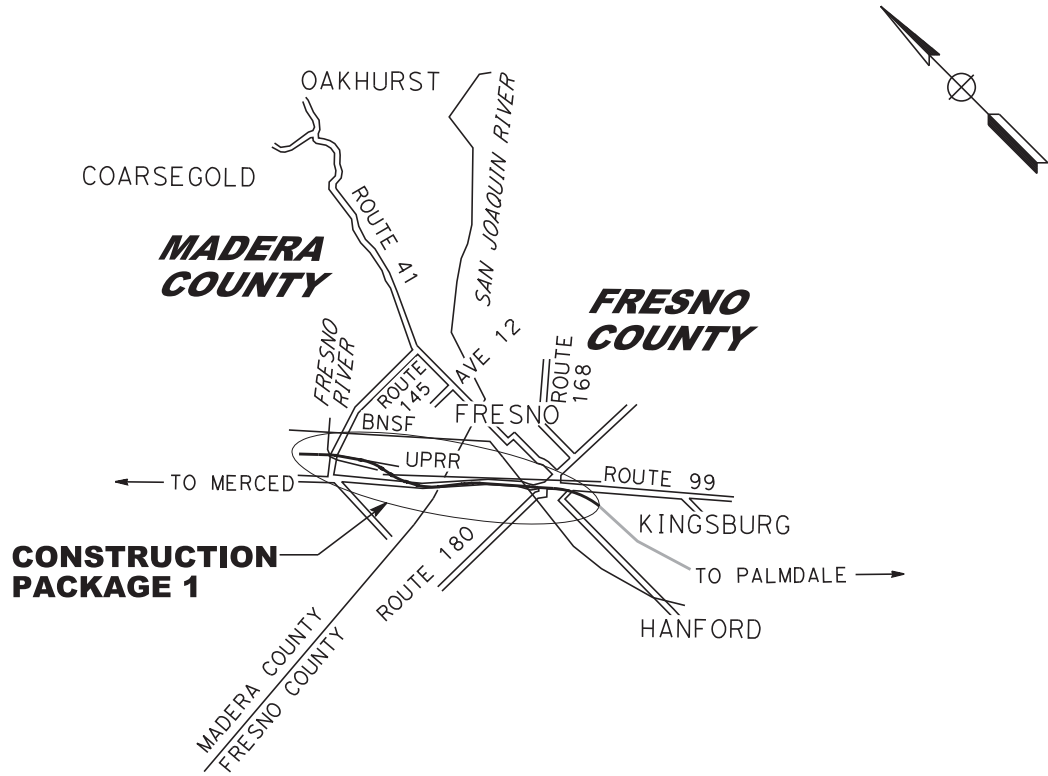


90% DESIGN SUBMITTAL  
CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1  
DOWNTOWN FRESNO VIADUCT

△ DENOTES REVISION



CONSTRUCTION PACKAGE 1 MAP



PROJECT LOCATION MAP

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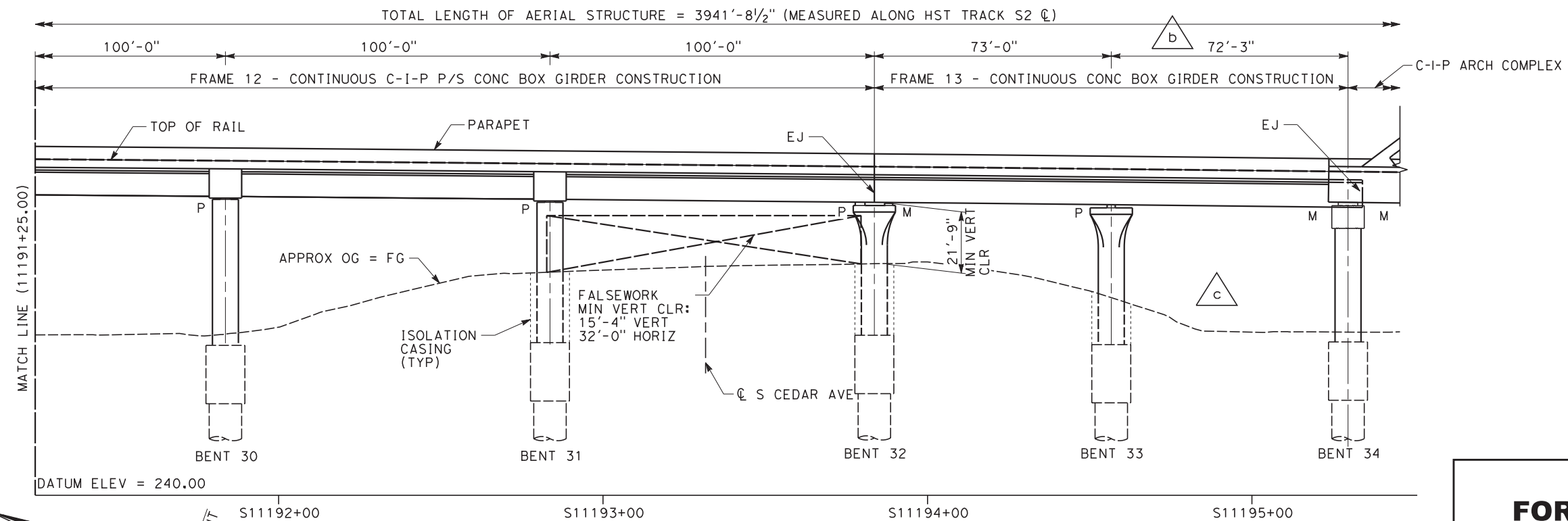
CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO. HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2 APPROVED SIGNED _____ DATE _____	C	09/14/16				90% DESIGN SUBMITTAL	DESIGNED BY D. GETTER	  	CONTRACT NO. HSR13-06
	c	08/05/16	DG	DJ	DJ	ESOC-003 - COLUMN FLARES, BEARINGS & ABUTMENTS	DRAWN BY J. GILMORE		DRAWING NO. ST-J0002-DFV
	b	04/08/16	DG	DJ	DJ	ESOC-002 - BENT FOUNDATIONS & COLUMNS	CHECKED BY D. JONES		SCALE NO SCALE
	a	11/13/15	SD	FN	SD	ESOC - 001 - FOUNDATIONS ONLY	IN CHARGE D. JONES		SHEET NO.
	B	08/15/15				60% DESIGN SUBMITTAL	DATE 09/14/2016		
REV	DATE	BY	CHK	APP		DESCRIPTION			

# **Exhibit B**

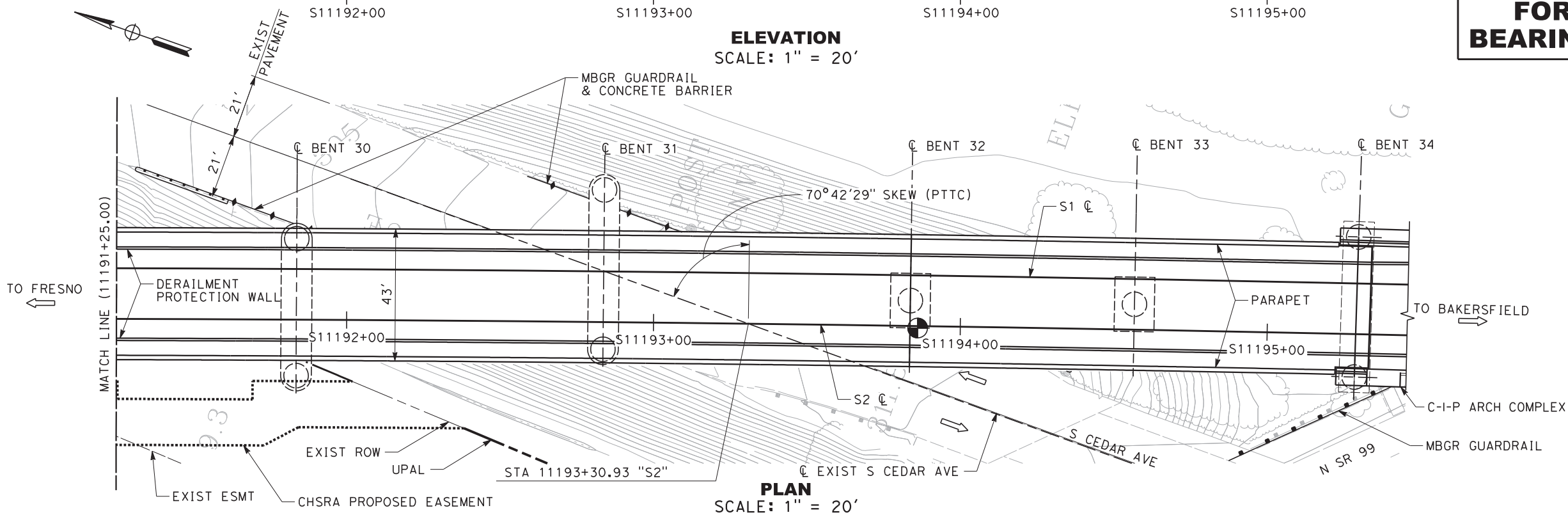
## Project Drawings

### **Exhibit B2 – Structure Drawings**

NOTE:  
1. FOR NOTES, TOP OF RAIL PROFILE, AND  
LEGEND, SEE "GENERAL PLAN AND  
ELEVATION SHEET 1 OF 12" SHEET.



**ESOC-003  
FOR COLUMN FLARES,  
BEARINGS AND ABUTMENTS**



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b	04/08/16	DG	DJ	DJ	ESOC-002 - BENT FOUNDATIONS & COLUMNS
a	11/13/15	SD	FN	SD	ESOC-001 - FOUNDATIONS ONLY
E	08/15/15				60% DESIGN SUBMITTAL
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY <b>D. GETTER</b>
DRAWN BY <b>J. GILMORE</b>
CHECKED BY <b>S. BAJZEK</b>
IN CHARGE <b>D. JONES</b>
DATE <b>09/14/2016</b>

**NOT FOR  
CONSTRUCTION**

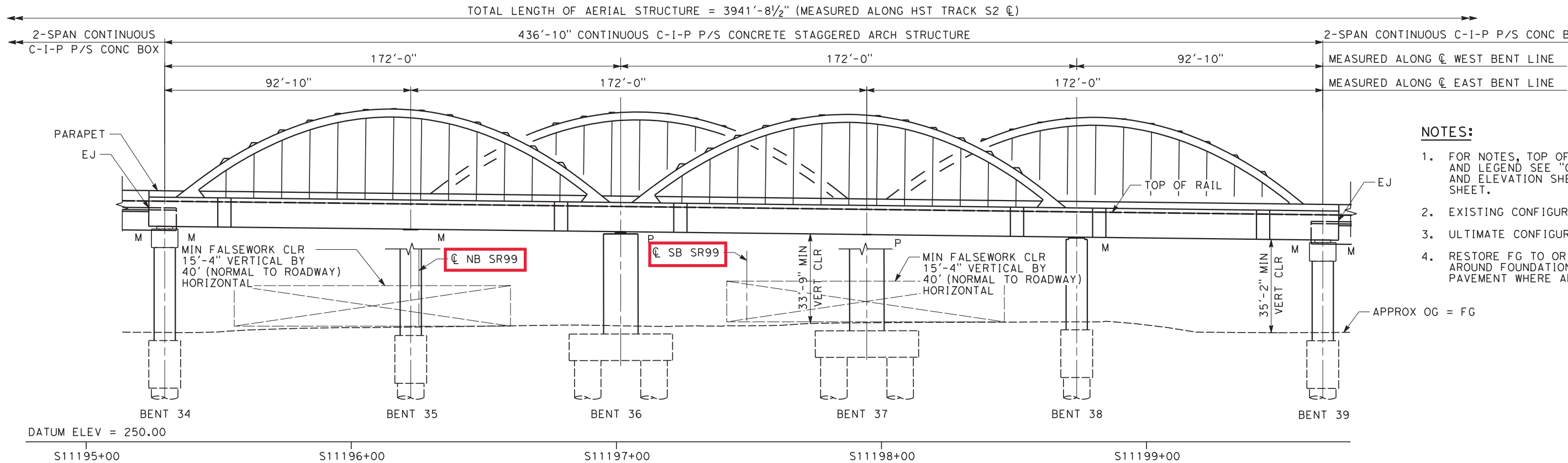
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APPROVED
SIGNED _____
DATE _____



**CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1**

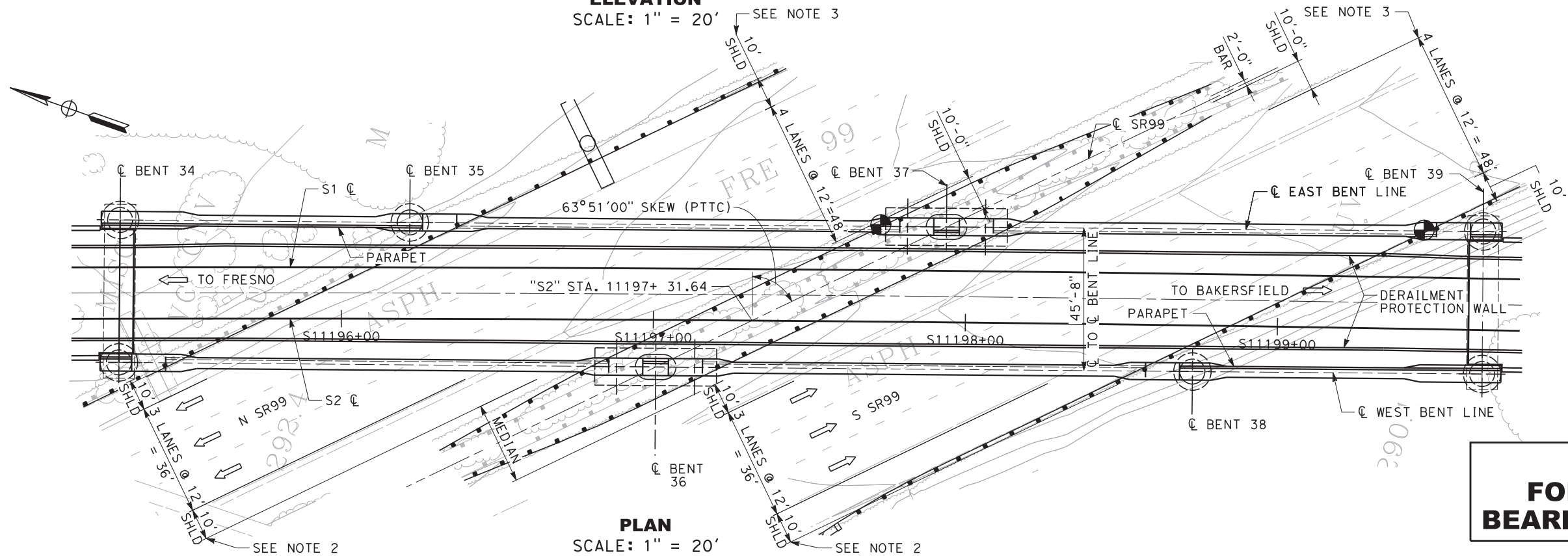
DOWNTOWN FRESNO VIADUCT  
GENERAL PLAN AND ELEVATION  
SHEET 7 OF 12

CONTRACT NO. <b>HSR13-06</b>
DRAWING NO. <b>ST-J1106-DFV</b>
SCALE <b>1" = 20'</b>
SHEET NO.



- NOTES:
1. FOR NOTES, TOP OF RAIL PROFILE, AND LEGEND SEE "GENERAL PLAN AND ELEVATION SHEET 1 OF 12" SHEET.
  2. EXISTING CONFIGURATION SHOWN.
  3. ULTIMATE CONFIGURATION BY OTHERS.
  4. RESTORE FG TO ORIGINAL CONDITIONS AROUND FOUNDATIONS INCLUDING PAVEMENT WHERE APPLICABLE.

ELEVATION  
SCALE: 1" = 20'



**ESOC-003  
FOR COLUMN FLARES,  
BEARINGS AND ABUTMENTS**

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b	08/05/16				ESOC-003 - COLUMN FLARES, BEARINGS & ABUTMENTS
a	04/08/16				ESOC-002 - BENT FOUNDATIONS & COLUMNS
E	08/15/15				60% DESIGN SUBMITTAL
D	07/02/15				DESIGN BASELINE REPORT PART 4 REV 1
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY S. SUN
DRAWN BY H. VARANECKAS
CHECKED BY E. JUMIC
IN CHARGE S. SUN
DATE 09/14/2016

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2

APPROVED

SIGNED \_\_\_\_\_

DATE \_\_\_\_\_



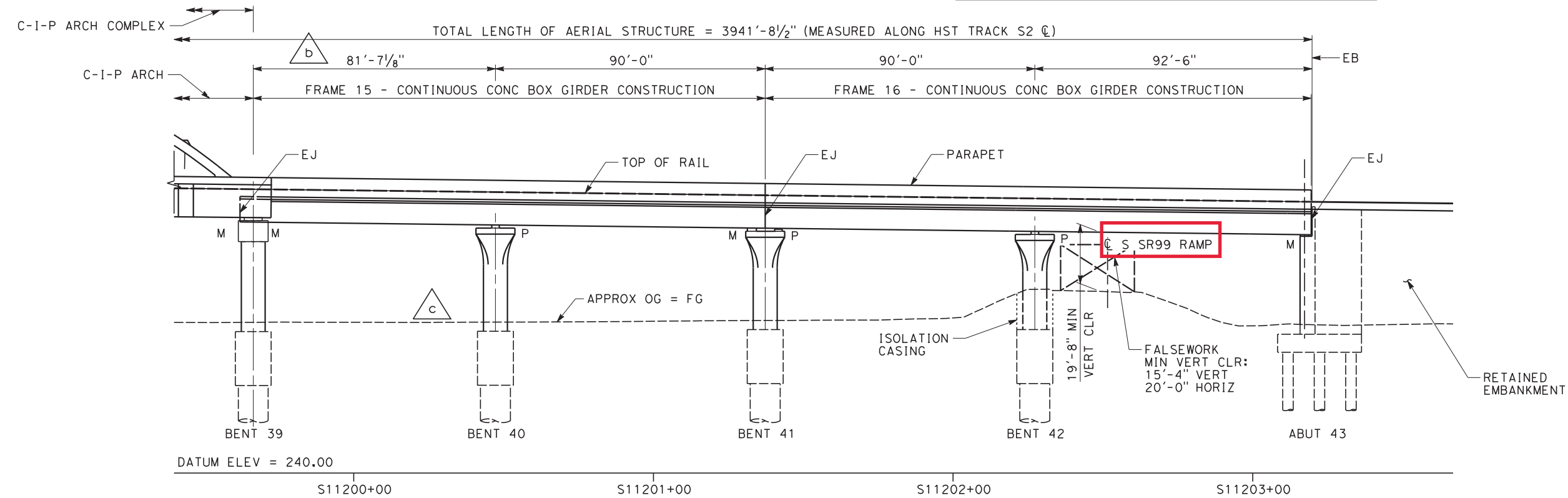
**CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1**

DOWNTOWN FRESNO VIADUCT  
GENERAL PLAN AND ELEVATION  
SHEET 8 OF 12

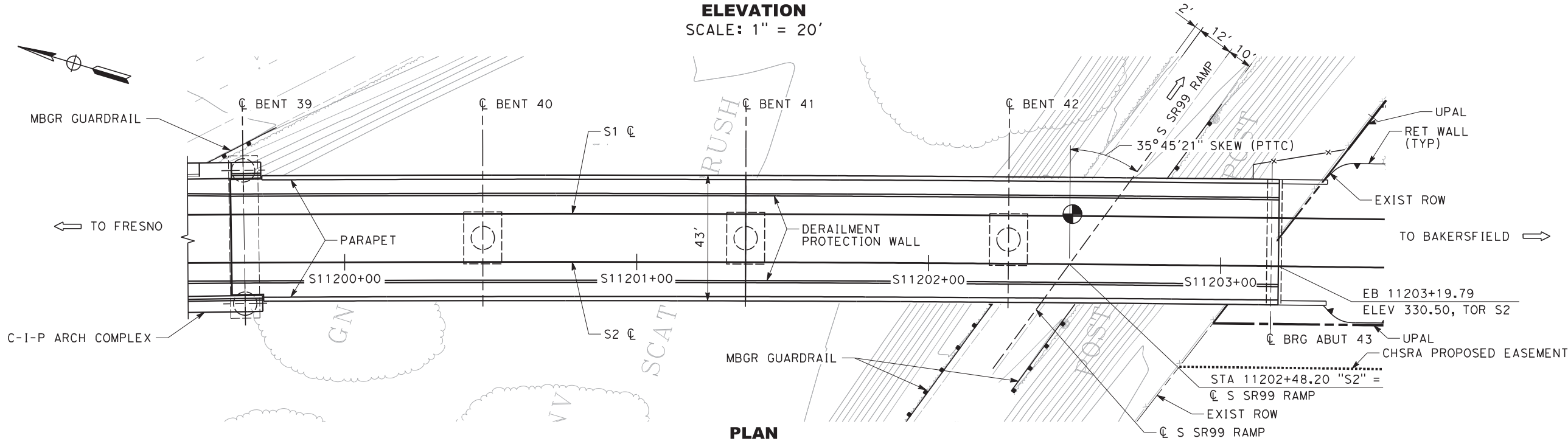
CONTRACT NO. HSR13-06
DRAWING NO. ST-J1107-DFV
SCALE 1" = 20'
SHEET NO.

**ESOC-003**  
**FOR COLUMN FLARES,**  
**BEARINGS AND ABUTMENTS**

**NOTE:**  
1. FOR NOTES, TOP OF RAIL PROFILE, AND  
LEGEND, SEE "GENERAL PLAN AND  
ELEVATION SHEET 1 OF 12" SHEET.



**ELEVATION**  
SCALE: 1" = 20'



**PLAN**  
SCALE: 1" = 20'

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c	08/05/16	DG	SB	DJ	ESOC-003 - COLUMN FLARES, BEARINGS & ABUTMENTS
b	04/08/16	DG	DJ	DJ	ESOC-002 - BENT FOUNDATIONS & COLUMNS
a	11/13/15	SD	FN	SD	ESOC-001 - FOUNDATIONS ONLY
E	08/15/15				60% DESIGN SUBMITTAL
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY <b>D. GETTER</b>
DRAWN BY <b>J. GILMORE</b>
CHECKED BY <b>S. BAJZEK</b>
IN CHARGE <b>D. JONES</b>
DATE <b>09/14/2016</b>

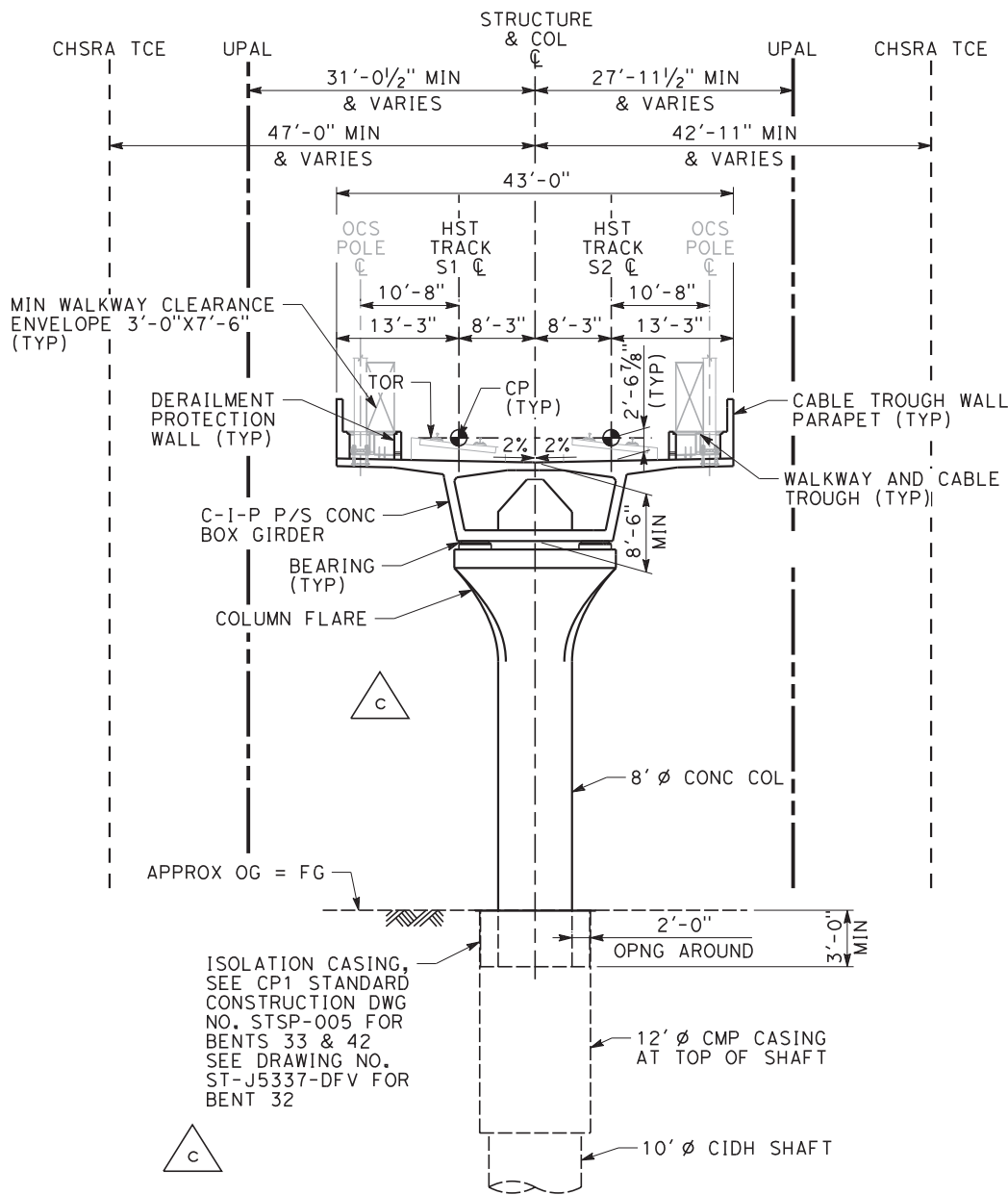
**NOT FOR  
CONSTRUCTION**

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2
APPROVED
SIGNED _____
DATE _____

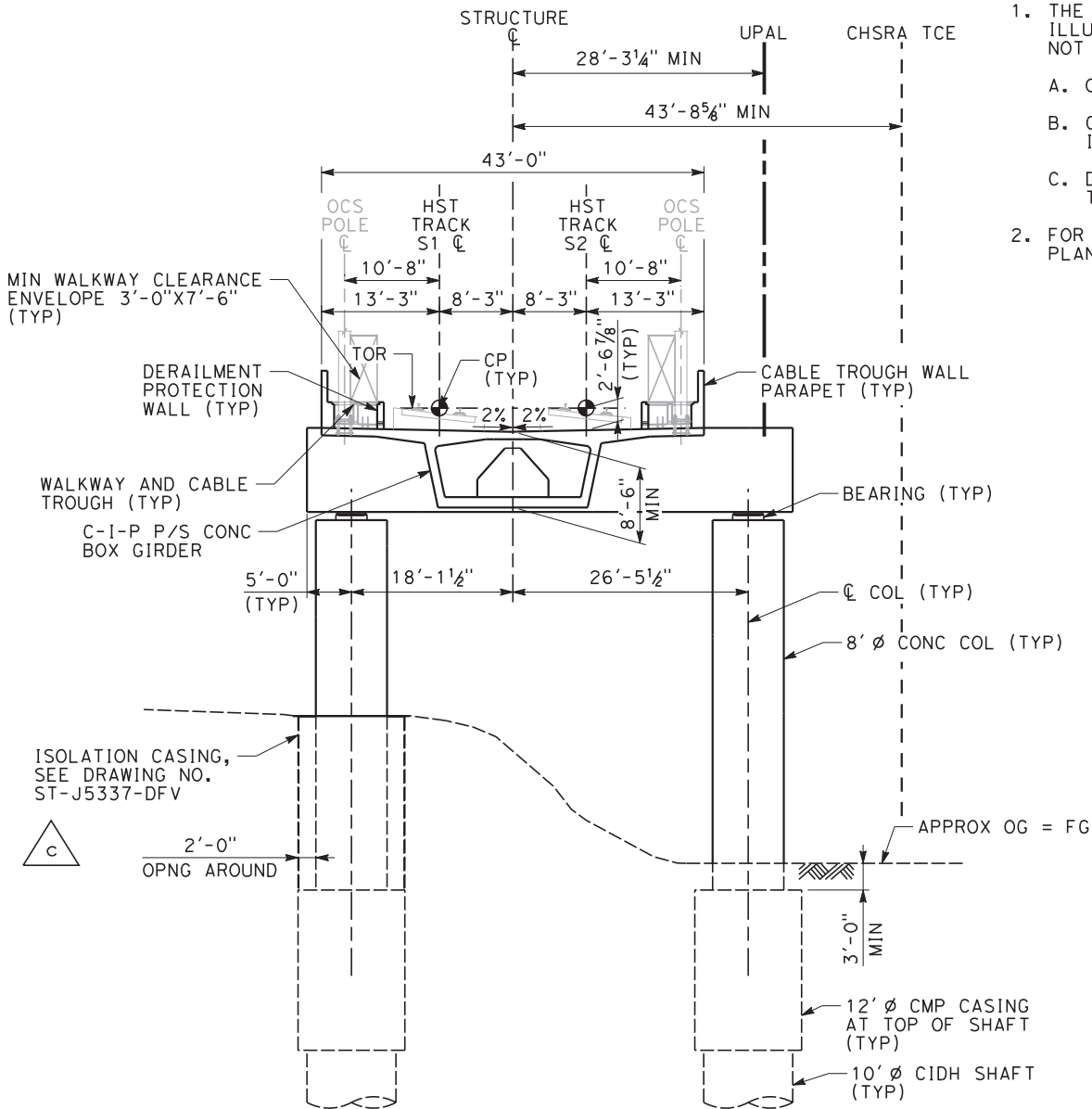


<b>CALIFORNIA HIGH-SPEED TRAIN PROJECT CONSTRUCTION PACKAGE 1</b>	CONTRACT NO. HSR13-06
DOWNTOWN FRESNO VIADUCT GENERAL PLAN AND ELEVATION SHEET 9 OF 12	DRAWING NO. ST-J1108-DFV
	SCALE 1" = 20'
	SHEET NO.

- NOTE:
1. THE FOLLOWING ITEMS ARE SHOWN FOR ILLUSTRATION PURPOSES ONLY AND ARE NOT INCLUDED IN THE CONTRACT:  
A. OCS POLES, BASE PLATES AND GROUT PAD.  
B. CABLE TROUGH PRE-CAST COVERS, INTERMEDIATE WALLS AND BASE SLABS.  
C. DIRECT FIXATION RAIL SYSTEM AND TRACK SLAB.
  2. FOR OTHER NOTES AND LEGEND, SEE "GENERAL PLAN AND ELEVATION SHEET 1 OF 12" SHEET.



SECTION AT TYPICAL VIADUCT BENT  
SCALE: 1" = 10'



SECTION AT BENT 30  
SCALE: 1" = 10'

**ESOC-003  
FOR COLUMN FLARES,  
BEARINGS AND ABUTMENTS**

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F	09/14/16				90% DESIGN SUBMITTAL
c	08/05/16	DG	SB	DJ	ESOC-003 - COLUMN FLARES, BEARINGS & ABUTMENTS
b	04/08/16	DG	DJ	DJ	ESOC-002 - BENT FOUNDATIONS & COLUMNS
a	11/13/15	SD	FN	SD	ESOC-001 - FOUNDATIONS ONLY
E	08/15/15				60% DESIGN SUBMITTAL
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY D. GETTER
DRAWN BY J. GILMORE
CHECKED BY S. BAJZEK
IN CHARGE D. JONES
DATE 09/14/2016

NOT FOR  
CONSTRUCTION

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2
APPROVED
SIGNED _____
DATE _____

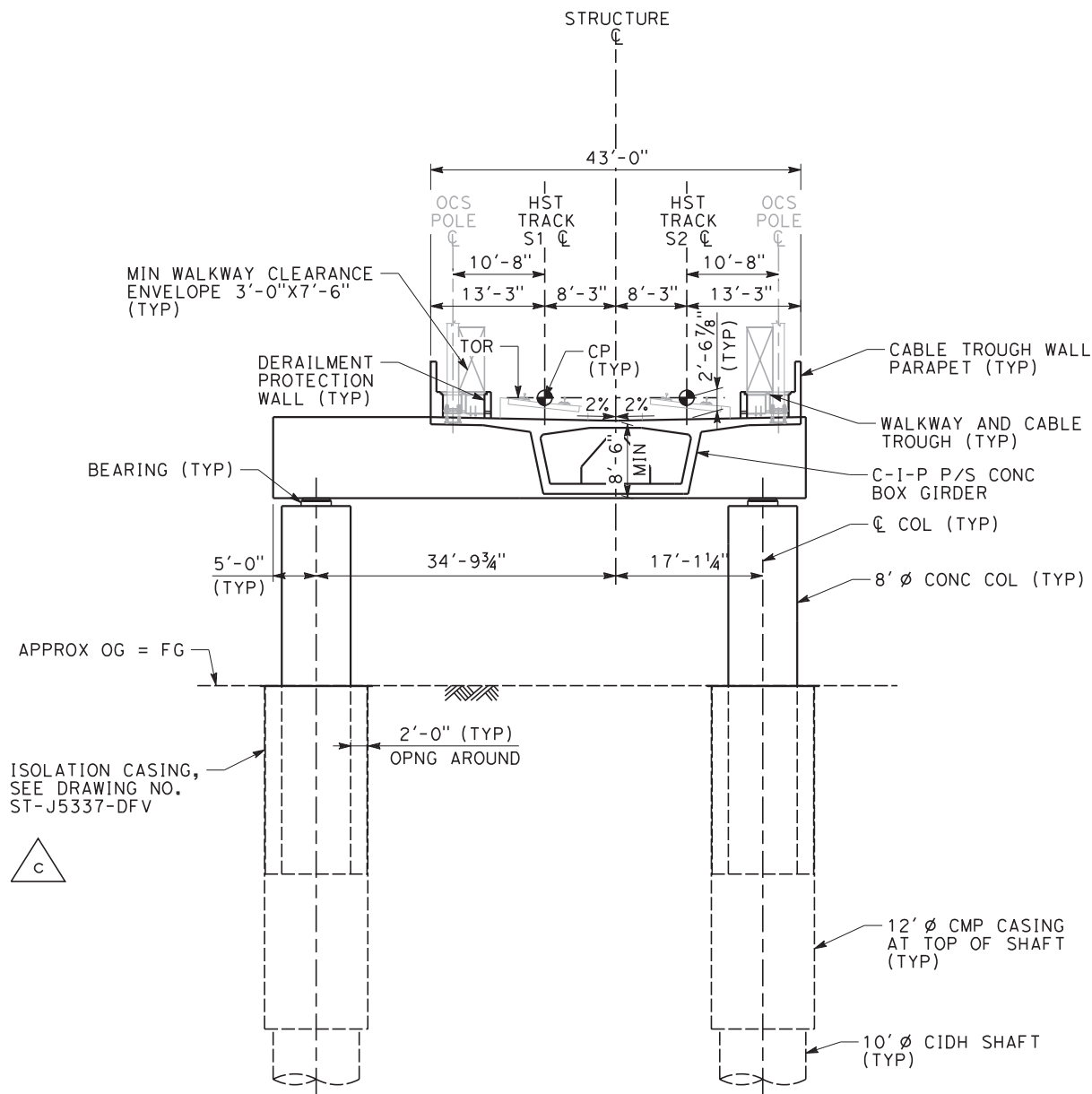


**CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1**

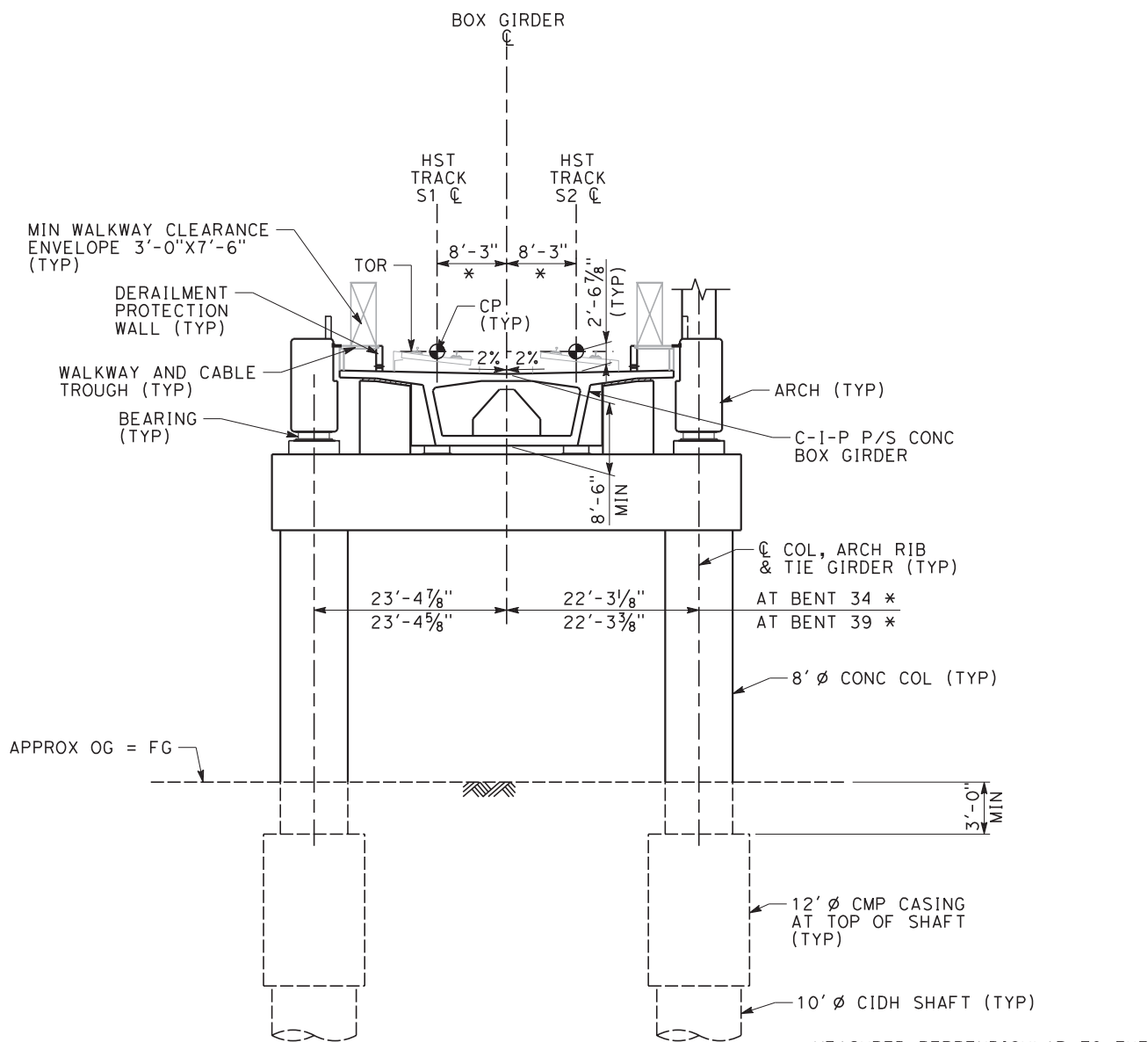
DOWNTOWN FRESNO VIADUCT  
GENERAL PLAN AND ELEVATION  
SHEET 10 OF 12

CONTRACT NO. HSR13-06
DRAWING NO. ST-J1109-DFV
SCALE 1" = 10'
SHEET NO.

NOTE:  
1. FOR NOTES, SEE "GENERAL PLAN AND ELEVATION SHEET 10 OF 12" SHEET.



SECTION AT BENT 31  
SCALE: 1" = 10'



SECTION AT BENT 34  
BENT 39 SIMILAR, OTHERWISE NOTED  
SCALE: 1" = 10'

**ESOC-003**  
**FOR COLUMN FLARES,**  
**BEARINGS AND ABUTMENTS**

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c	08/05/16	DG	SB	DJ	ESOC-003 - COLUMN FLARES, BEARINGS & ABUTMENTS
b	04/08/16	DG	DJ	DJ	ESOC-002 - BENT FOUNDATIONS & COLUMNS
a	11/13/15	SD	FN	SD	ESOC-001 - FOUNDATIONS ONLY
E	08/15/15				60% DESIGN SUBMITTAL
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY D. GETTER
DRAWN BY J. GILMORE
CHECKED BY S. BAJZEK
IN CHARGE D. JONES
DATE 09/14/2016

NOT FOR  
CONSTRUCTION

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2
APPROVED
SIGNED _____
DATE _____

Tutor Perini

ZACHRY

PARSONS  
A joint venture



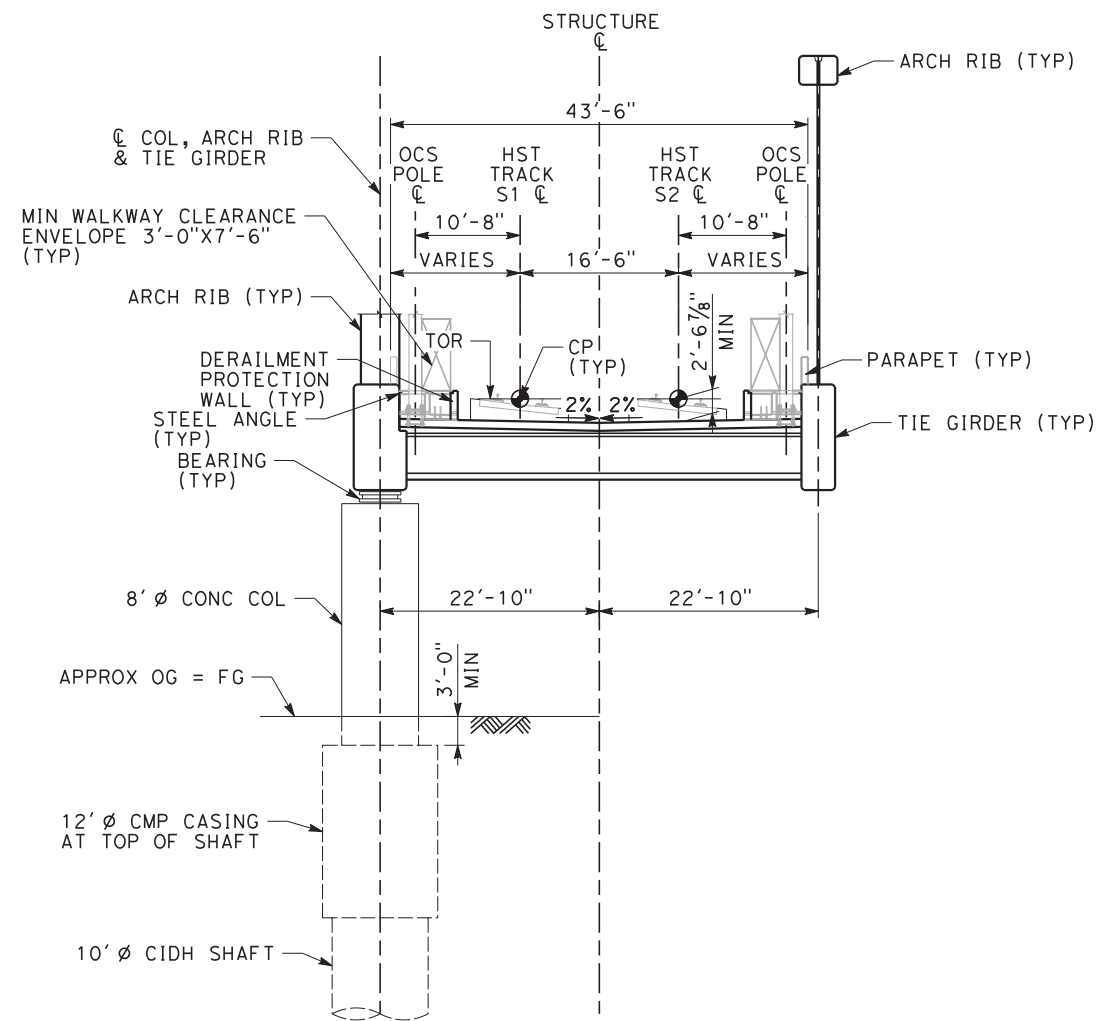
CALIFORNIA  
HIGH-SPEED RAIL AUTHORITY

CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1

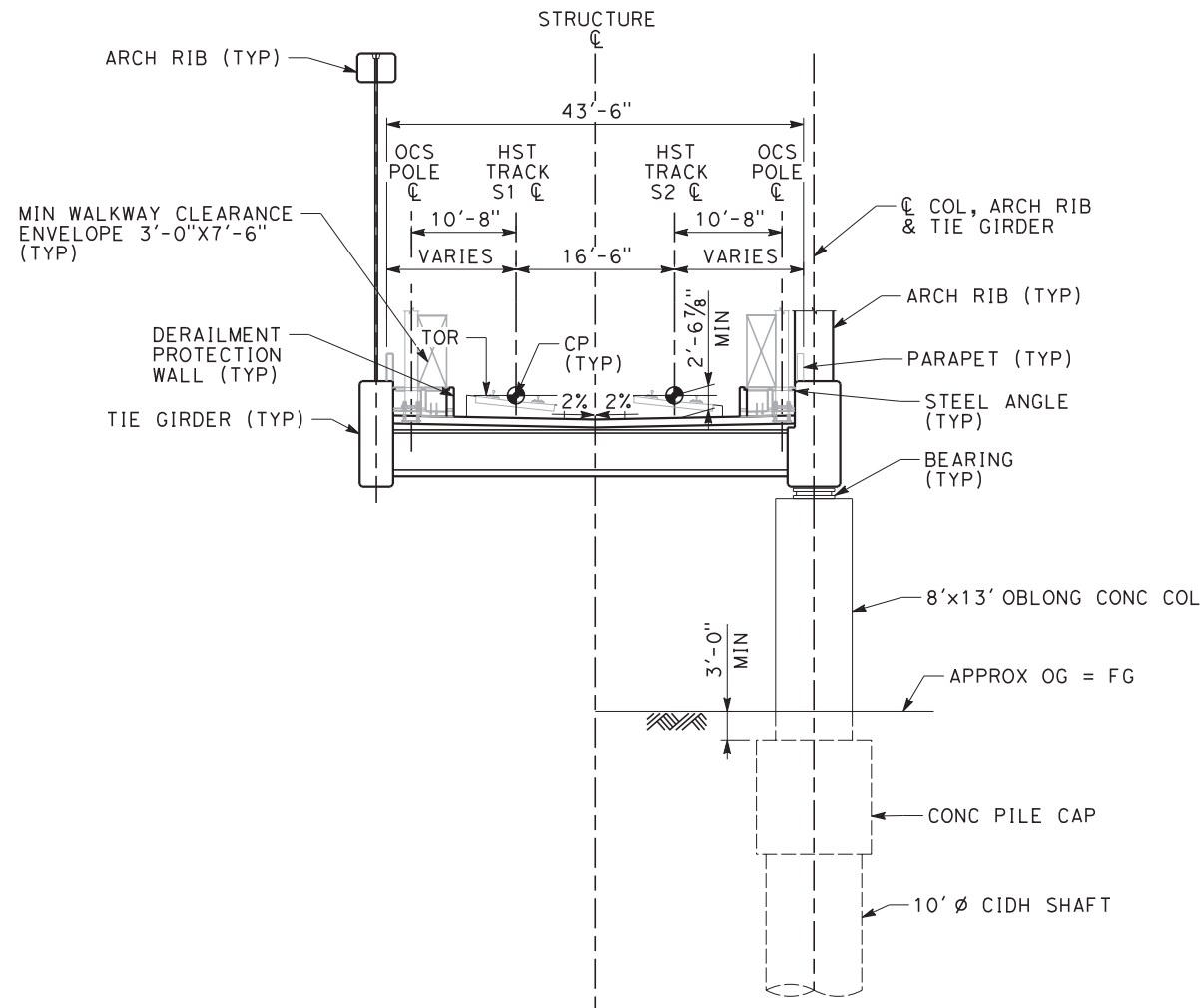
DOWNTOWN FRESNO VIADUCT  
GENERAL PLAN AND ELEVATION  
SHEET 11 OF 12

CONTRACT NO. HSR13-06
DRAWING NO. ST-J1110-DFV
SCALE 1" = 10'
SHEET NO.

NOTE:  
1. FOR NOTES, SEE "GENERAL PLAN AND ELEVATION SHEET 10 OF 12" SHEET.



**SECTION AT BENT 35**  
BENT 38 SIMILAR, OPPOSITE HAND  
SCALE: 1" = 10'



**SECTION AT BENT 36**  
BENT 37 SIMILAR, OPPOSITE HAND  
SCALE: 1" = 10'

**ESOC-003**  
**FOR COLUMN FLARES,**  
**BEARINGS AND ABUTMENTS**

**FID: S-AS-195.1**

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9/16

36671

F	09/14/16				90% DESIGN SUBMITTAL	DESIGNED BY K. PATEL	<div>CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2</div> <div>APPROVED</div> <div>SIGNED _____</div> <div>DATE _____</div>	<div><div>TutorPerini</div><div>ZACHRY</div><div>PARSONS</div><div>A joint venture</div></div> <div>CALIFORNIA HIGH-SPEED RAIL AUTHORITY</div>	<div>CALIFORNIA HIGH-SPEED TRAIN PROJECT CONSTRUCTION PACKAGE 1</div> <div>DOWNTOWN FRESNO VIADUCT GENERAL PLAN AND ELEVATION SHEET 12 OF 12</div>	<div>CONTRACT NO. HSR13-06</div> <div>DRAWING NO. ST-J1111-DFV</div> <div>SCALE 1" = 10'</div> <div>SHEET NO.</div>
b	08/05/16				ESOC-003 - COLUMN FLARES, BEARINGS & ABUTMENTS	DRAWN BY H. VARANECKAS				
a	04/08/16				ESOC-002 - BENT FOUNDATIONS & COLUMNS	CHECKED BY E. JUMIC				
E	08/15/15				60% DESIGN SUBMITTAL	IN CHARGE S. SUN				
D	07/02/15				DESIGN BASELINE REPORT PART 4 REV 1	DATE 09/14/2016				
REV	DATE	BY	CHK	APP	DESCRIPTION					

# **Exhibit B**

## Project Drawings

### **Exhibit B3 – Guideway Drawings**

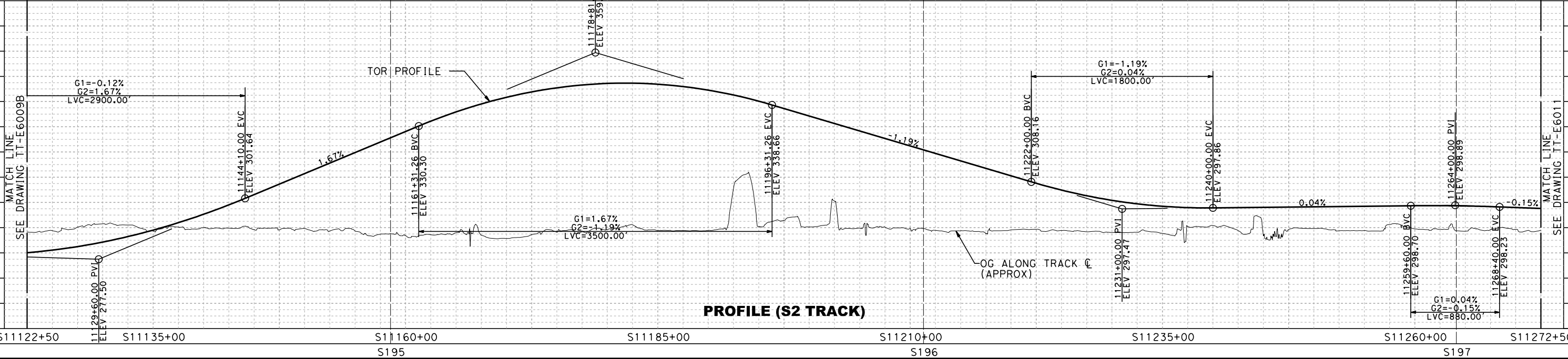
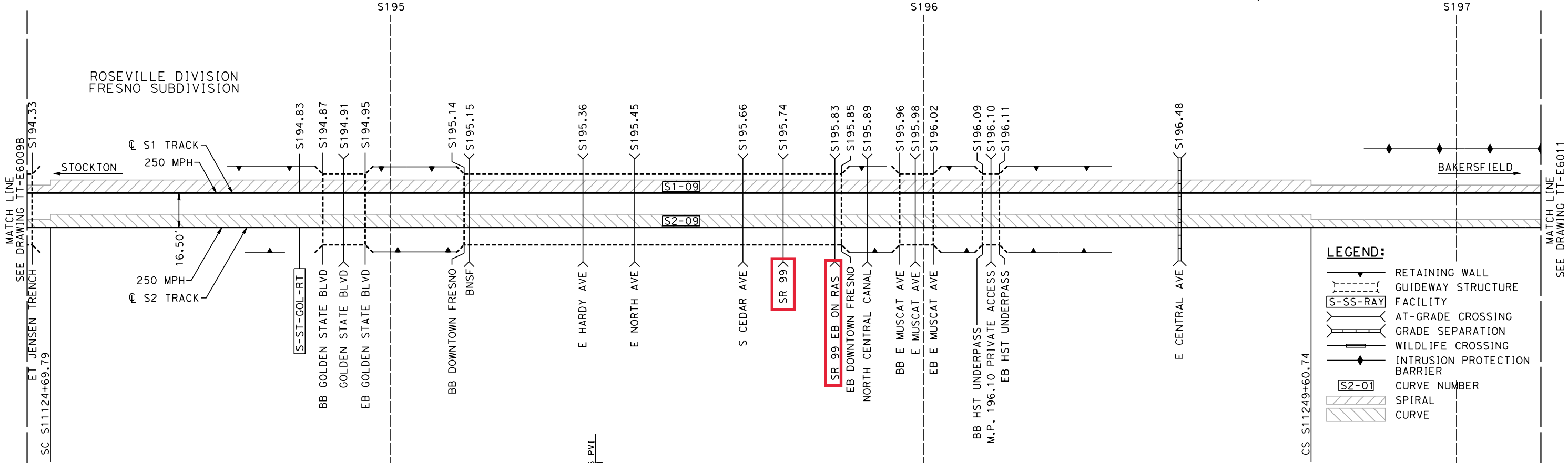
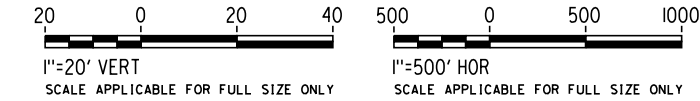
CITY OF FRESNO

ALIGNMENT

NOTE:

INFORMATION SHOWN ON TRACK CHART OTHER THAN TRACKWORK SHALL BE USED AS GUIDANCE, AND IS FOR INFORMATION ONLY.

11/18/2016 8:58:07 AM CHSTP.TB1 CHSR\_PDF\_half\_black.plt c:\pwworking\california\p002494c\dms07895\TT-E6010.dgn



					DESIGNED BY S. WILSON	90% SUBMITTAL	NOT FOR CONSTRUCTION	APPROVED SIGNED _____ DATE _____		CALIFORNIA HIGH-SPEED TRAIN PROJECT CONSTRUCTION PACKAGE 1		TRACK CHART SHEET 2 OF 3		CONTRACT NO. HSR13-06 DRAWING NO. TT-E6010 SCALE AS SHOWN SHEET NO.	
					DRAWN BY R. PATEL										
					CHECKED BY A. FERNANDEZ										
					IN CHARGE C. CUSHNIE										
REV	DATE	BY	CHK	APP	DESCRIPTION	11/28/2016									
B	11/28/16				90% GUIDEWAY STA 11030+00.00 TO 11299+98.90										
A	10/09/15				60% GUIDEWAY STA 11030+00.00 TO 11299+98.90										



						DESIGNED BY R. PATEL	90% SUBMITTAL  NOT FOR CONSTRUCTION	CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2  APPROVED  SIGNED _____  DATE _____	<div>Tutor Perini</div> <div>ZACHRY</div> <div>PARSONS A joint venture</div>	<div></div> <div>CALIFORNIA HIGH-SPEED RAIL AUTHORITY</div>	CONTRACT NO. HSR13-06	
D	11/28/16				90% GUIDEWAY STA 11030+00.00 TO 11299+98.90	DRAWN BY R. OYEWOLE						DRAWING NO. TT-D1101
C	10/09/15				60% GUIDEWAY STA 11030+00.00 TO 11299-98.90	CHECKED BY E. CASTELINO						SCALE AS SHOWN
B	07/02/15				DESIGN BASELINE REPORT PART 4 REV 1	IN CHARGE C. CUSHNIE						SHEET NO.
A	01/30/15				DESIGN BASELINE REPORT PART 4 REV 0	DATE 11/28/2016						
REV	DATE	BY	CHK	APP	DESCRIPTION							

CALIFORNIA HIGH-SPEED TRAIN PROJECT CONSTRUCTION PACKAGE 1	
TRACK GUIDEWAY STA S11186+00 TO S11200+00 PLAN AND PROFILE	

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p003773a

# CITY OF FRESNO

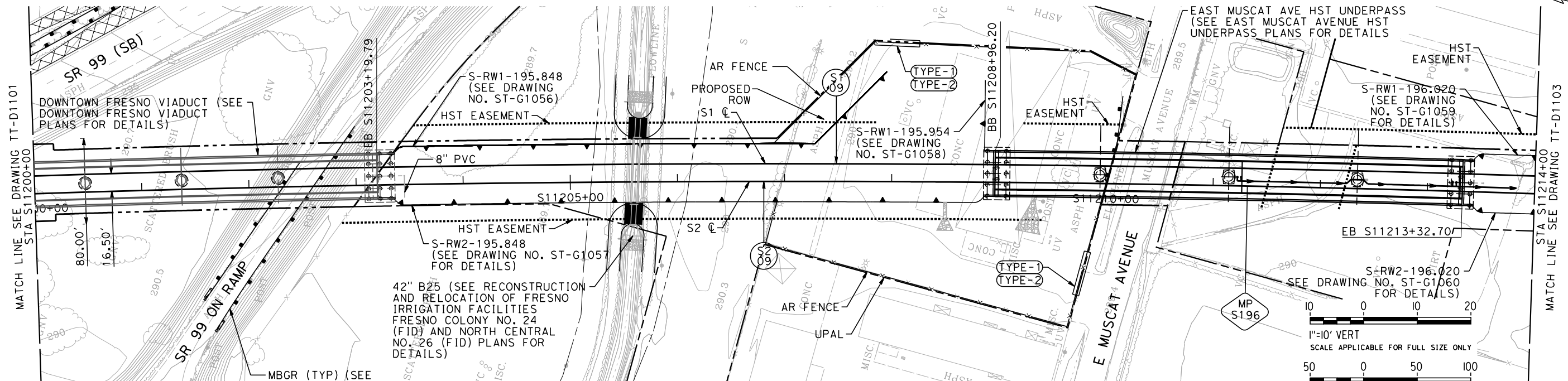
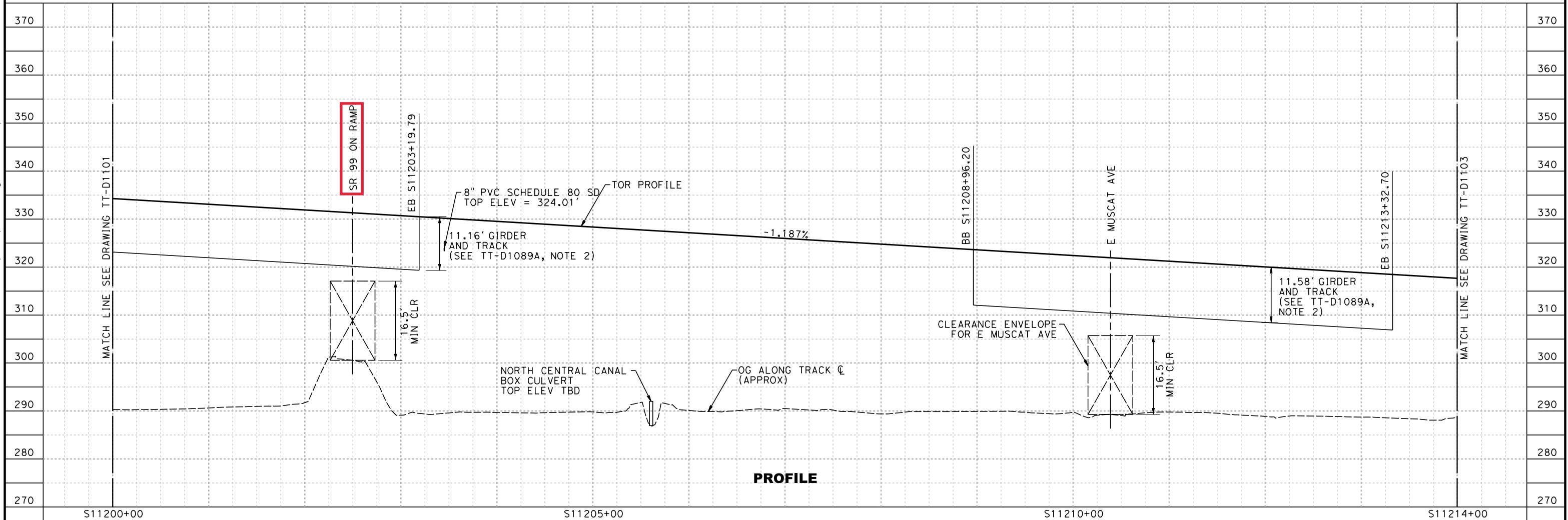


Exhibit B3-  
page 10

PLAN



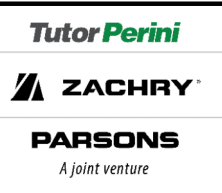
PROFILE

REV	DATE	BY	CHK	APP	DESCRIPTION
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C	10/09/15				60% GUIDEWAY STA 11030+00.00 TO STA 11299+98.90
B	07/02/15				DESIGN BASELINE REPORT PART 4 REV 1
A	01/30/15				DESIGN BASELINE REPORT PART 4 REV 0

DESIGNED BY <b>R. PATEL</b>
DRAWN BY <b>R. OYEWOLE</b>
CHECKED BY <b>E. CASTELINO</b>
IN CHARGE <b>C. CUSHNIE</b>
DATE <b>11/28/2016</b>

90% SUBMITTAL  
  
NOT FOR  
CONSTRUCTION

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO. 4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2
APPROVED
SIGNED _____
DATE _____



**CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1**

TRACK GUIDEWAY  
STA S11200+00 TO S11214+00  
PLAN AND PROFILE

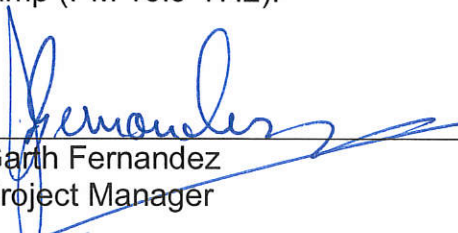
CONTRACT NO. <b>HSR13-06</b>
DRAWING NO. <b>TT-D1102</b>
SCALE <b>AS SHOWN</b>
SHEET NO.

# **Exhibit C**

Local Project Support

Caltrans Concurrence

I, **Garth Fernandez**, an employee of the **California Department of Transportation (Caltrans)** and authorized to sign this letter of agreement on its behalf, hereby declare that Caltrans concurs with the proposed CP1 Project at the Downtown Fresno Viaduct as described in the Supplemental Project Report dated February 3, 2017 for the HST encroachments at E. North/S. Cedar Avenues, State Route 99, and Southbound On-Ramp (PM 16.9-17.2).



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Garth Fernandez  
Project Manager

Date

3/27/2017

California Department of Transportation  
2015 E. Shields Ave, Suite 100  
Fresno, CA 93726

# Exhibit D

## Scoping Memo

### Scoping Memo Information for Applications

#### A. Category (Check the category that is most appropriate)

☐ **Adjudicatory** - “Adjudicatory” proceedings are: (1) enforcement investigations into possible violations of any provision of statutory law or order or rule of the Commission; and (2) complaints against regulated entities, including those complaints that challenge the accuracy of a bill, but excluding those complaints that challenge the reasonableness of rates or charges, past, present, or future, such as **formal rough crossing complaints** (maximum 12 month process if hearings are required).

☒ **Ratesetting** - “Ratesetting” proceedings are proceedings in which the Commission sets or investigates rates for a specifically named utility (or utilities), or establishes a mechanism that in turn sets the rates for a specifically named utility (or utilities). “Ratesetting” proceedings include complaints that challenge the reasonableness of rates or charges, past, present, or future. Other proceedings may also be categorized as ratesetting when they do not clearly fit into one category, such as **railroad crossing applications** (maximum 18 month process if hearings are required).

☐ **Quasi-legislative** - “Quasi-legislative” proceedings are proceedings that establish policy or rules (including generic ratemaking policy or rules) affecting a class of regulated entities, including those proceedings in which the Commission investigates rates or practices for an entire regulated industry or class of entities within the industry.

#### B. Are hearings necessary?

☐

Yes

☒

No

If yes, identify the material disputed factual issues on which hearings should be held, and the general nature of the evidence to be introduced. Railroad crossing applications which are not controversial usually do not require hearings.

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Are public witness hearings necessary? ☐ Yes ☒ No

Public witness hearings are set up for the purpose of getting input from the general public and any entity that will not be a party to the proceeding. Such input usually involves presenting written or oral statements to the presiding officer, not sworn testimony. Public witness statements are not subject to cross-examination.

**C. Issues** - List here the specific issues that need to be addressed in the proceeding.  
None

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**D. Schedule (Even if you checked “No” in B above)** Should the Commission decide to hold hearings, indicate here the proposed schedule for completing the proceeding within 12 months (if categorized as adjudicatory) or 18 months (if categorized as ratesetting or quasi-legislative).

The schedule should include proposed dates for the following events as needed:

<u>August 23, 2017</u>	Filing Date
<u>September 22, 2017</u>	Comment Period
<u>December 22, 2017</u>	Proposed Decision (4 months from filing)
<u>February 23, 2018</u>	Final Decision (6 months from filing)

If hearings unexpectedly becomes necessary:

<u>February 23, 2018</u>	Prehearing conference
<u>May 24, 2018</u>	Hearings
<u>August 22, 2018</u>	Briefs due
<u>September 21, 2018</u>	Submission
<u>December 20, 2018</u>	Proposed decision (90 days after submission)
<u>February 22, 2019</u>	Final decision (60 days after proposed decision is mailed)

# Exhibit E

## Verification

I am an officer of the California High-Speed Rail Authority, Applicant herein, and am authorized to make this verification on its behalf. The contents of this document are true of my own knowledge, except as matters that are stated on information or belief, and as to those matters, I believe them to be true.

Dated this 28<sup>th</sup> day of July, 2017 at Sacramento, California.

Signed



Bruce W. Armistead  
Director of Operations and Maintenance  
California High-Speed Rail Authority  
770 L Street, Suite 620  
Sacramento, CA 95814  
Tel: (916) 324-1541