



**FILED**

03/17/26

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A2603016

**BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA**

Application of the California High-Speed Rail Authority to construct an overhead grade separation at Avenue 17 over BNSF's existing track (MP 1021.42); and proposed high-speed tracks (MP 168.67) within Madera County, California.

Application No. \_\_\_\_\_

**APPLICATION**

The California High-Speed Rail Authority (CHSRA or Applicant) hereby respectfully requests authority from the California Public Utilities Commission (CPUC or Commission) to construct proposed tracks and a highway-rail overhead grade separation in the County of Madera at Avenue 17, which is part of the proposed California High-Speed Train System (CHSTS).

In support of this application the Applicant asserts that:

1. The Applicant's exact legal name is California High-Speed Rail Authority and CHSRA is a public agency formed pursuant to the California High-Speed Rail Act (California Public Utilities Code § 185000 *et seq.*).
2. Applicant's principal place of business is at California High-Speed Rail Authority, 770 L Street, Suite 620; Sacramento, CA 95814. The main telephone number is (916) 324-1541.
3. All correspondence, communication, notices, orders and other papers relative to this application should be addressed to: Dominique Rulens, Acting Director of Rail and Operations, at the above address and at the following e-mail address:

[Dominique.Rulens@hsr.ca.gov](mailto:Dominique.Rulens@hsr.ca.gov). Mr. Rulens is an employee of CHSRA and Applicant's authorized agent for this purpose.

4. The proposed CHSRA mainline tracks and overhead grade separation structure at Avenue 17 are a part of the "North Extension" of the California High-Speed Train System Construction Package 1 ("CP1 Project"). The CP1 Project is the first construction package of the CHSTS. CHSRA is working cooperatively with local stakeholders—government agencies and railroads—along the corridor.

The CP1 Project limits begin in Madera County in the north and extend south through the City of Fresno, into unincorporated Fresno County. The CP1 Project length is approximately thirty miles and consists of two CHSTS mainline tracks, generally aligned in a north-south direction.

Grade separations allow for safe and simultaneous movement of highway and rail modes of travel. With that consideration, the proposed highway-rail and rail-rail crossings of the CP1 Project include nineteen (19) new grade separations, modification of thirteen (13) existing at-grade crossings to grade separations, and modification of six (6) existing grade separations. Eight (8) existing at-grade crossings and one (1) existing grade separation are proposed to be closed. In addition, the North Extension includes one (1) new grade separation and modification of two (2) existing at-grade crossings to grade separations. No new at-grade crossings are proposed for the CP1 Project.

For the protection of the general public and the CHSTS passengers, the CHSTS will be a closed corridor. Unauthorized access to CHSRA right-of-way will be protected with the use of a continuous Access Restricted (AR) fence or barrier. The typical AR fence consists of an eight foot (8 ft) welded wire mesh, which is more resistant to climbing and forced entry than chain link fence. When required by CHSRA criteria, expanded metal mesh fence or barrier walls will be used. In addition to the AR fence or barrier wall, intrusion monitoring and detection is proposed. CHSRA will also employ security services to enforce right-of-way access restrictions.

The overhead contact system, which will supply energy to the rail vehicles, will be consistent with the requirements of General Orders 26-D, 95, 128 and 176.

5. This application and relief sought herein is filed pursuant to Sections 1201 through 1205, inclusive, of the Public Utilities Code and in accordance with Rules 3.7 and 3.9 of the Commission’s Rules of Practice and Procedure.
6. The CHSRA, in cooperation with the BNSF Railway Company (“BNSF”) and Madera County, proposes to construct two CHSTS mainline tracks and an overhead grade separation structure at Avenue 17 in Madera County.

The CHSRA and BNSF mile posts, subdivisions, CPUC crossing numbers, and DOT crossing number for the Avenue 17 overhead grade separation are listed in the following table.

<b>Avenue 17</b>	<b>Mile Post</b>	<b>Proposed CPUC Xing #</b>	<b>DOT #</b>
CHSRA- Sierra Subdivision	168.67	135S-168.67-A	968518J
BNSF- Stockton Subdivision	1021.42	002-1021.42-A	

7. The legal description for the site of the proposed Avenue 17 grade separation (Rule 3.7(a)) is provided in Exhibit A, attached hereto and made a part hereof.
8. The crossing identification numbers of the nearest existing public crossings (Rule 3.7(b)) are as follows:

The nearest crossing to the north is at Road 27 and the BNSF’s Stockton Subdivision mainline track in the County of Madera, with CPUC No. 002-1022.50 and DOT No. 028619B.

The nearest crossing to the south is at Raymond Road and the BNSF’s Stockton Subdivision mainline track in the County of Madera, with CPUC No. 002-1020.40 and DOT No. 028618U.

9. CP1 Project design drawings showing the general vicinity (Rule 3.7 (e)), proposed horizontal and vertical alignment for the two CHSTS tracks (Rule 3.7 (f) and 3.9 (c)), roadway plan and profile (Rule 3.7 (d) and (f)), and structure design drawings (Rule 3.7 (d)) for the proposed crossing site are provided in Exhibit B, attached hereto and made a part hereof.

As shown on the design drawings and as summarized in the following table, the minimum permanent clearances exceed the clearances required by General Order 26-D. No temporary clearance exceptions are requested.

<b>Avenue 17 Clearances</b>	<b>Minimum Vertical</b>	<b>Minimum Horizontal</b>
CHSRA Permanent	28'-0" (Trk S2)	>10' to OCS Pole
CHSRA Temporary	N/A	N/A
BNSF Permanent	27'-5" (Trk ML)	>50' (ML to intrusion barrier)
BNSF Temporary	24'-0" (Trk ML)	>15'

10. In support of the proposed Avenue 17 grade separation letter of concurrence from the BNSF is provided in Exhibit C, attached hereto and made a part hereof. The agreements between the CHSRA and Madera County (Rule 3.9 (a)) were provided to the Commission as Exhibit C2 *Master Agreement* and Exhibit C3 *Settlement Agreement* to Application Number A.15-10-007 and are made a part hereof by reference. A Notice of Availability for the *Master Agreement* and *Settlement Agreement*, and Certificate of Service are filed with this application.
11. The CHSRA, as the lead agency under the provisions of the California Environmental Quality Act (CEQA), prepared the *Final Environmental Impact Report/ Environmental Impact Statement (Final EIR/EIS) for the Merced to Fresno Section* of the California High-Speed Train Project. The Federal Railroad Administration (FRA) was the lead agency under the National Environmental Policy Act (NEPA). The Notice of Determination (NOD) for the *Final EIR/EIS* was certified on May 3, 2012 (State Clearinghouse No. 2009091125) and Record of Decision (ROD) was dated September 18, 2012. The *Final EIR/EIS for the Merced to Fresno Section* of the California High-Speed Train System was provided to the Commission (Rule 2.4) on archival and standard CD-ROM/DVD as Exhibit D to Application Number A.15-10-007. A Notice of Availability for the *Final EIR/EIS* and Certificate of Service are filed with this application.
12. Attached hereto as Exhibit D is the Scoping Memo for this application.
13. Attached hereto as Exhibit E is the Verification for this application.

WHEREFORE, the Applicant respectfully requests:

1. That the Public Utilities Commission of the State of California issue an Order authorizing construction of the Avenue 17 overhead grade separation, and CHSTS mainline tracks, pursuant to the provisions of Sections 1201-1205, inclusive, of California Public Utilities Code.
2. That the Order provides seventy-two (72) months from the date of such an Order within which to complete the work requested. The extended period of time is requested due to the unique nature of the CP1 Project.

The CP1 Project scope includes design and construction of the structures and track roadbed, encompassing nearly 50 crossings. To complete construction at the Avenue 17 crossing and all of the crossings along the length of the CP1 Project, CHSRA anticipates issuing a separate contract within several years for construction of the proposed tracks and overhead contact system. The construction of the CP1 Project and the future track work contract will not be complete within the standard 36-month authorization period but is likely to be completed within 72 months. In addition, since the proposed crossings are grade separations, the clearances and crossing configurations are well defined and not subject to the variability common for at-grade crossings. Given the number of new CHSTS crossing requests that will be submitted to the CPUC in the coming months and years, authorizing the extended period for this crossing will relieve both the CP1 Project and CPUC staff of preparing and processing numerous crossing extensions that will result with a 36-month Order.

Signed \_\_\_\_\_



Dominique Rulens  
Acting Director of Rail and Operations  
California High-Speed Rail Authority  
770 L Street, Suite 620  
Sacramento, CA 95814  
Tel: (916) 908-1277  
E-Mail: [Dominique.Rulens@hsr.ca.gov](mailto:Dominique.Rulens@hsr.ca.gov)

# List of Application Exhibits

- Exhibit A**      **Crossing Legal Descriptions**
- Exhibit B**      **Project Drawings**
  - B1 – Vicinity Map
  - B2 – Structure Drawings
  - B3 – Roadway Drawings
  - B4 – Guideway Drawings
- Exhibit C**      **Local Project Support**
  - BNSF Concurrence Letter
- Exhibit D**      **Scoping Memo**
- Exhibit E**      **Verification**

## Supporting Documentation

### **Notice of Availability**

Exhibit C2 to Application A. 15-10-007  
*Master Agreement*

Exhibit C3 to Application A. 15-10-007  
*Settlement Agreement*

Exhibit D to Application A. 15-10-007  
*Final Environmental Impact Report / Environmental Impact Statement for the  
Merced to Fresno Section of the California High-Speed Train Project*

### **Certificate of Service**

# **Exhibit A**

## Crossing Legal Descriptions

Avenue 17 Overhead

LEGAL DESCRIPTION

That portion of Avenue 17 in the southeast quarter of Section 6 and the northeast quarter of Section 7, all in Township 11 South, Range 18 East, Mount Diablo Meridian, described as follows:

**COMMENCING** at a 3/4" iron pipe with tag stamped "LS4546" marking the southwest corner of said Section 6, as shown on Parcel Map 1792, filed February 14, 1980 in Book 26 of Maps at Page 80, Madera County Records; thence along the south line of said Section 6, North 89°56'21" East, 2705.71 feet; thence continuing along said south line, North 89°57'09" East, 822.89 feet to the **POINT OF BEGINNING**; thence leaving said south line, North 44°31'33" West, 42.04 feet to the northerly line of the Avenue 17 right of way; thence along said northerly line, North 89°57'11" East, 166.55 feet to the westerly line of the Atchison, Topeka and Santa Fe Railroad right of way; thence along said westerly line, South 44°31'08" East, 84.08 feet to the southerly line of the Avenue 17 right of way; thence along said southerly line, South 89°57'09" West, 166.80 feet; thence leaving said southerly line, North 44°31'33" West, 42.04 feet to the **POINT OF BEGINNING**.

Lands abutting said rail shall have no right or easement of access thereto.

Bearings and distances are based on the California Coordinate System of 1983, Zone 3, as shown on Record of Survey, Book 60, Pages 130-131, Madera County Records, Distances are U.S. Survey Feet. Divide grid distances shown hereinabove by 1.00000334 to obtain ground level distances.

END DESCRIPTION

This real property description has been prepared by me, or under my direction, in conformance with the requirements of the Professional Land Surveyor's Act.

Steven G. Kuehter  
Professional Land Surveyor  
California No. 8057

*Steven G. Kuehter*  
Date



EXHIBIT "A"  
LEGAL DESCRIPTION

The land referred to herein below is situated in the County of Madera, State of California and is described as follows:

MF-20-1239-1 (EASEMENT)

Being a portion of the Burlington Northern Santa Fe Railroad Right of Way, lying in Section 6 and Section 7, Township 11 South, Range 18 East, Mount Diablo Meridian, said portion described as follows:

**COMMENCING** at a 2" iron pipe tagged LS 2737 set for the northeast corner of said Section 7; thence along the north line of said Section 7, South 89°57'09" West, 1575.38 feet to the northeasterly line of said Burlington Northern, Santa Fe Railroad Right of Way and the **POINT OF BEGINNING**; thence southeasterly into said Section 7 and along said northeasterly line, South 44°31'08" East, 42.04 feet; thence leaving said northeasterly line, South 89°57'09" West, 140.14 feet to the southwesterly line of said Burlington Northern, Santa Fe Right of Way; thence along said southwesterly line, North 44°31'08" West, 42.04 feet to the north line of said Section 7; thence continuing along said southwesterly line, into said Section 6, North 44°31'08" West, 42.04 feet; thence leaving said southwesterly line, North 89°57'09" East, 140.14 feet to the northeasterly line of said Burlington Northern, Santa Fe Right of Way; thence along said northeasterly line, South 44°31'08" East, 42.04 feet to the **POINT OF BEGINNING**.

Bearings and distances are based on the California Coordinate System of 1983, Zone 3, as shown on Record of Survey, Book 60, Pages 130-131, Madera County Records. Distances are U.S. Survey Feet. Distances are grid distances. Divide grid distances shown hereinabove by 1.00000334 to obtain ground level distances.

END DESCRIPTION

This land description has been prepared by me, or under my direction, in conformance with the requirements of the Professional Land Surveyor's Act.



Erwin D Backlin II  
Professional Land Surveyor  
California No. L7765

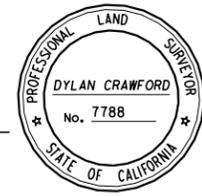


6/21/2016

Date

COUNTY OF MADERA  
T.11S. R.18E. M. D. M.  
SECTION 6

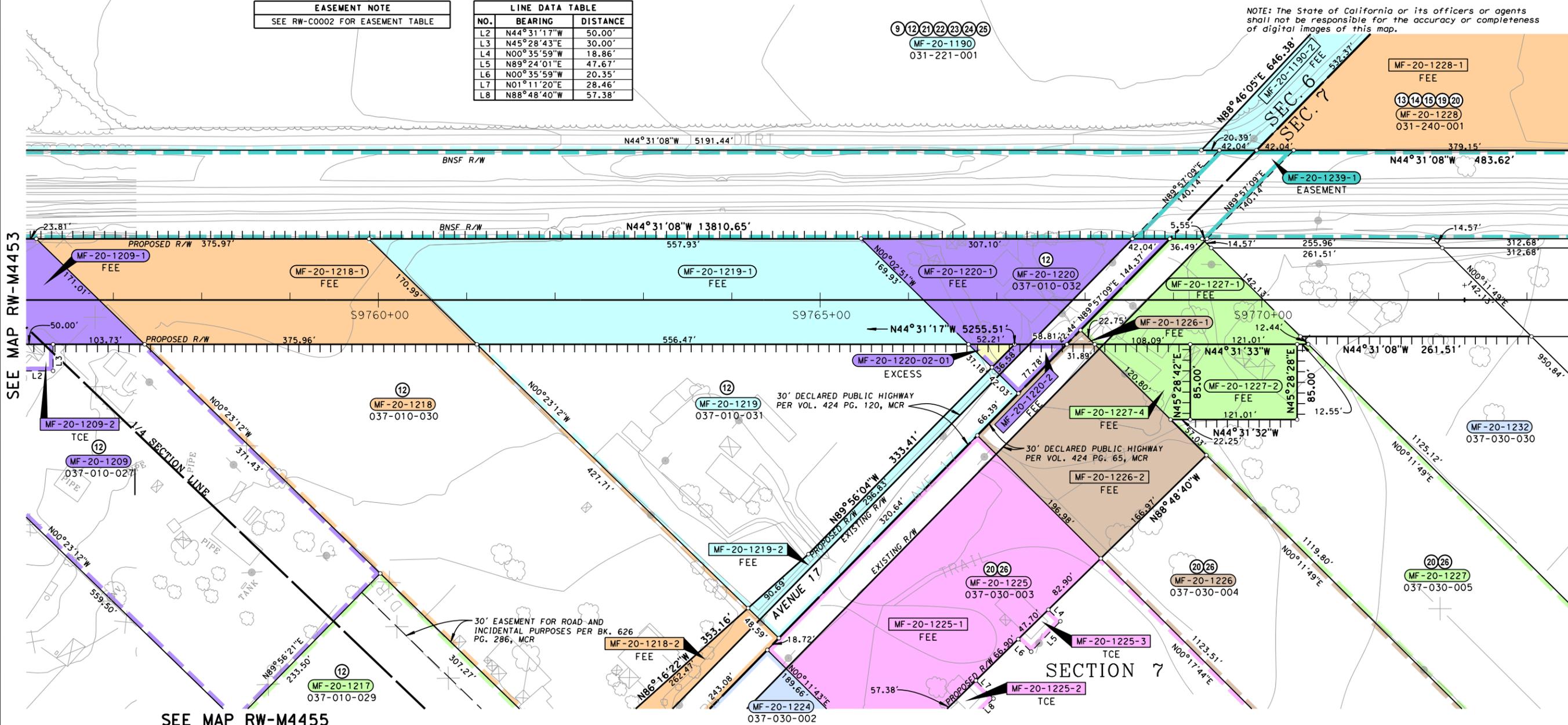
Dylan Crawford  
PROFESSIONAL LAND SURVEYOR  
06-17-2016  
DATE



EASEMENT NOTE  
SEE RW-C0002 FOR EASEMENT TABLE

NO.	BEARING	DISTANCE
L2	N44°31'17"W	50.00'
L3	N45°28'43"E	30.00'
L4	N00°35'59"W	18.86'
L5	N89°24'01"E	47.67'
L6	N00°35'59"W	20.35'
L7	N01°11'20"E	28.46'
L8	N88°48'40"W	57.38'

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.



SEE MAP RW-M4453

SEE MAP RW-M4455

SEE MAP RW-M4456

11/8/2016 9:02:39 AM CHSR-RW-Appraisal.tbl CHSR-RW-PDF\_half.plt Y:\Jobs\21301 CHSR-CP1D\MicroStation MF Maps\Sheets\15-RW-M4454-Z3.dgn

DATE	BY	REVISIONS
06/23/16	D. CRAWFORD	ADDED MF-20-1190-3
07/20/16	D. CRAWFORD	ADDED MULTIPLE TCES
11/07/16	D. CRAWFORD	REMOVED MF-20-1190-3

PARCEL#	TITLE CODE	GRANTOR/GRANTEE	AREAS (SQUARE FEET OR AS NOTED)				REMARKS	RECORDATION	
			TOTAL	REQUIRED (U)F	EXCESS (U)F	REMAINDER		TYPE (3)	DATE (4)DOC.#
MF-20-1190-2	F	HERMAN 2008 REVOCABLE TRUST	20,829,143	99,966		20,536,306			
MF-20-1209-1	F	ELIZABETH LEE	130,493	17,106		113,387			
MF-20-1209-2	TCE	ELIZABETH LEE		1,500					
MF-20-1218-1	F	JUAN C. VIDAL	196,724	44,768		144,827			
MF-20-1218-2	F	JUAN C. VIDAL		7,129 [7,854]					
MF-20-1219-1	F	JEWEL RODRIGUEZ	162,335	66,338		90,927			
MF-20-1219-2	F	JEWEL RODRIGUEZ		5,070 [11,614]					
MF-20-1220-1	F	VICTOR RUIZ	23,573	22,384 [5,004]	680	0			
MF-20-1220-2	F	VICTOR RUIZ		509 [1,892]					
MF-20-1224-1	F	MICHAEL AND ROSE MARY PADILLA	436,505	52,911 [10,180]		383,594			
MF-20-1226-1	F	LEON, CAROLYNE, AND EUGENE TEASLEY	216,493	[255]		188,340			
MF-20-1226-2	F	LEON, CAROLYNE, AND EUGENE TEASLEY		28,153 [4,999]					
MF-20-1227-1	F	PAULINE GOREE ET AL	219,406	13,452 [4,741]		190,154			
MF-20-1227-2	F	PAULINE GOREE ET AL		10,286					
MF-20-1227-4	F	PAULINE GOREE ET AL		5,514 [26]					
MF-20-1239-1	E	BNSF		8,408					

GRANTOR NOTES		NOTES	
①	Areas shown exclude underlying fee in the adjoining public way.	COORDINATES, BEARINGS AND DISTANCES ARE BASED ON THE CALIFORNIA COORDINATE SYSTEM OF 1983, ZONE 3	
	Ac=Acres, SF=Square Feet	COORDINATE SYSTEM OF 1983, ZONE 3	
	Indicates Underlying Fee (UF) Area	EPOCH 2007.0, DISTANCES ARE U.S. SURVEY FEET UNLESS OTHERWISE NOTED	
	Indicates Indeterminate UF	DISTANCES AND STATIONING ARE GRID DISTANCES.	
	TITLE CODES:	DIVIDE DISTANCES BY 1.00000334 TO OBTAIN GROUND LEVEL DISTANCES.	
	A=Access Rights Only		
	F=Fee		
	E=Easement (Ease)		
	TCE=Temp Construction Ease		
	TC=Other Temp Ease (See Remarks)		
	O=Other (See Remarks)		
	TYPE:		
	GD=Grant deed		
	ED=Easement deed		
	QC=Quitclaim		
	DD=Director's deed		
	DE=Director's easement deed		
	DK=Director's quitclaim deed		
	FOC=Final Order of Condemnation		
	HE=Highway easement deed		
	REL=Relinquishment		
	VAC=Vacation		
	JUA=Joint use agreement		
	CCUA=Consent to common use agreement		
④	Document or Instrument number		

CALIFORNIA HIGH-SPEED TRAIN PROJECT  
MERCED TO MADERA

**RIGHT OF WAY APPRAISAL MAP RW-M4454**

CONTRACT	COUNTY	PM	DWG No.	SHEET NO.	TOTAL SHEETS
MAD			15-RW-M4454-23	20	23

# **Exhibit B**

## **Project Drawings**

<b>Exhibit B1 – Vicinity Map</b>	<b>Pg 1</b>
<b>Exhibit B2 – Structure Drawings</b>	<b>Pg 2</b>
<b>Exhibit B3 – Roadway Drawings</b>	<b>Pg 4</b>
<b>Exhibit B4 – Guideway Drawings</b>	<b>Pg 12</b>

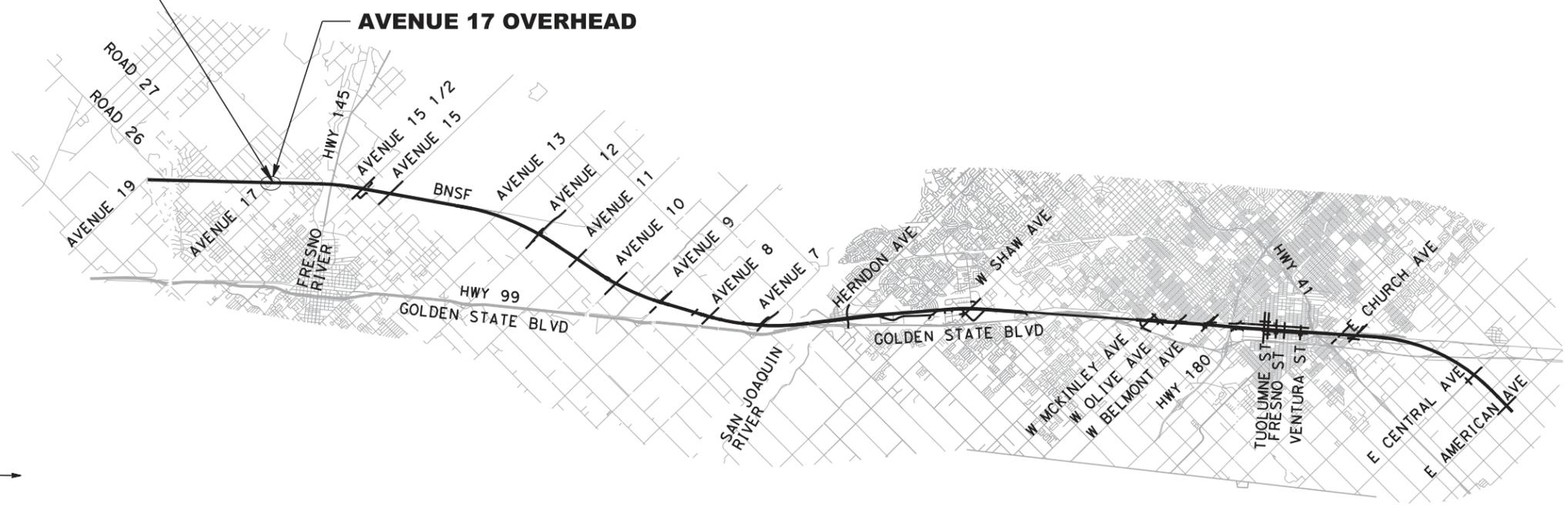


**CALIFORNIA**  
HIGH-SPEED RAIL AUTHORITY

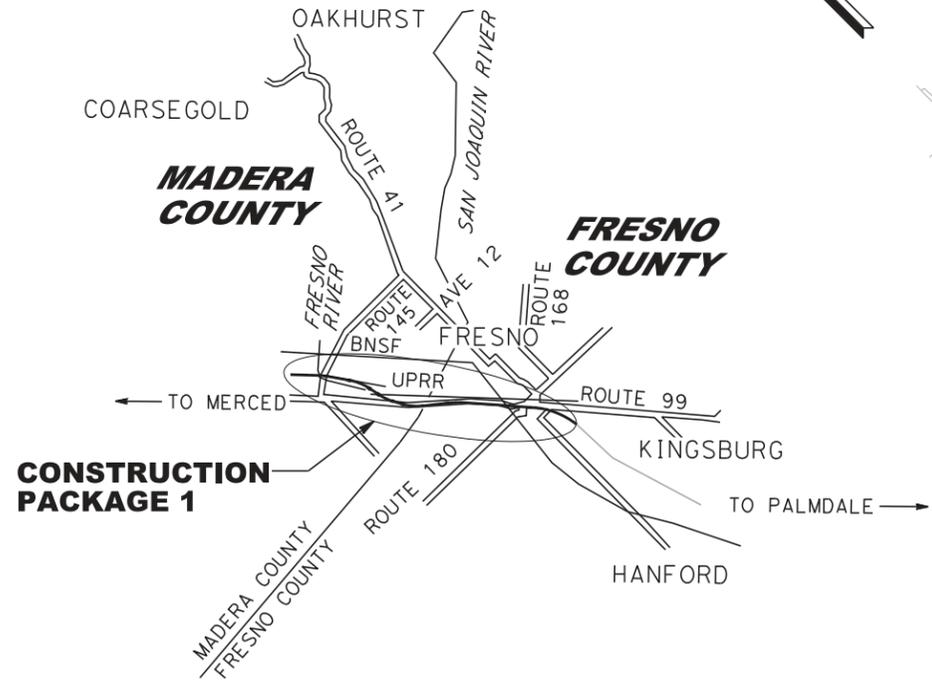


**READY FOR CONSTRUCTION**  
**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**  
**AVENUE 17 OVERHEAD**

**BEGIN/END WORK**  
**STA S9767+81.04**



**CONSTRUCTION PACKAGE 1 MAP**



**PROJECT LOCATION MAP**

**FID: S-OH-168.7**

	CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO. HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2 APPROVED					DESIGNED BY <b>V. TRINH</b>		CONTRACT NO. HSR13-06
	SIGNED _____ DATE _____	0 08/30/19 VT AB VT	READY FOR CONSTRUCTION	DRAWN BY <b>T. DOUNG</b>	DRAWING NO. CP1E-ST-K0002-A17			
	REV DATE BY CHK APP DESCRIPTION					CHECKED BY <b>A. BANESH</b>		SCALE NO SCALE
						IN CHARGE <b>V. TRINH</b>		SHEET NO. 1
						DATE 08/30/2019	A joint venture	

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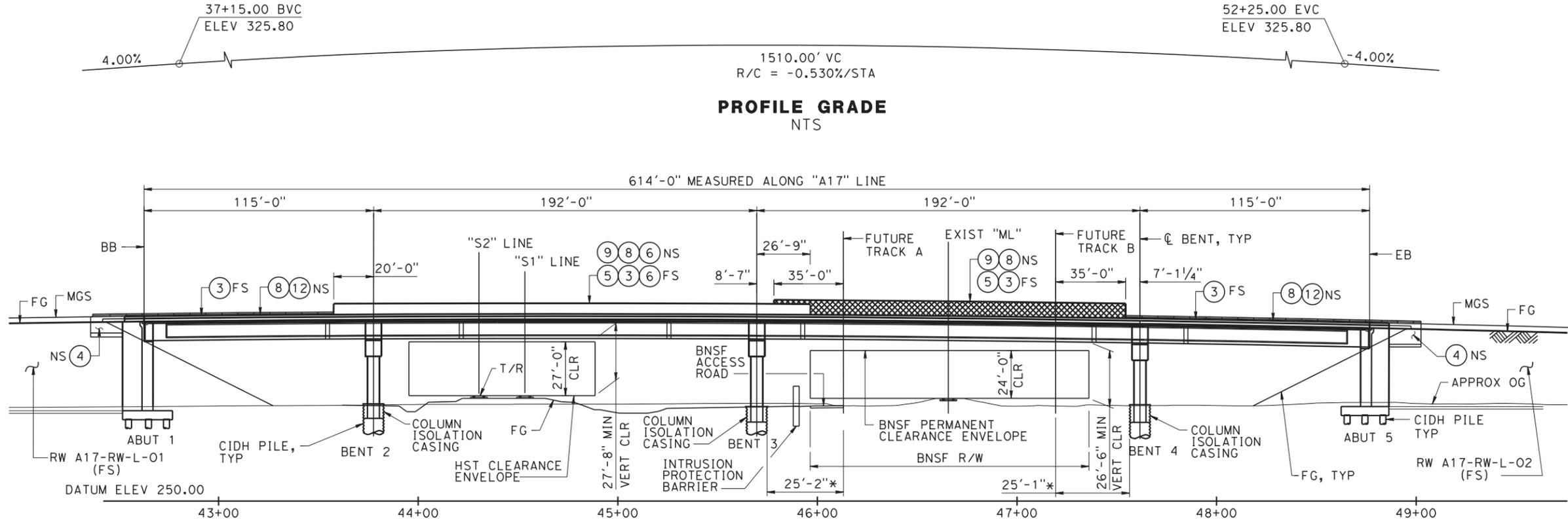
**NOTES:**

- ① PAINT "AVENUE 17 OVERHEAD"
- ② PAINT "BRIDGE NO. S-OH-168.7"
- ③ CONCRETE BARRIER TYPE 732
- ④ RETAINING WALL
- ⑤ CHAIN LINK RAILING TYPE 7 (MOD)
- ⑥ FUTURE OPAQUE SOLID PLATE (NIC) WITHIN LIMITS OF HST ROW
- ⑧ CONCRETE BARRIER TYPE 26 (MOD)
- ⑨ CHAIN LINK RAILING TYPE 3 (MOD)
- ⑩ CONCRETE BARRIER TYPE 732 (MOD)
- ⑪ DECK DRAIN TYPE D-1(MOD)
- ⑫ TUBULAR HAND RAILING
- ⑭ POLYESTER CONCRETE
- ⑮ CONCRETE BARRIER TYPE 732B (MOD)

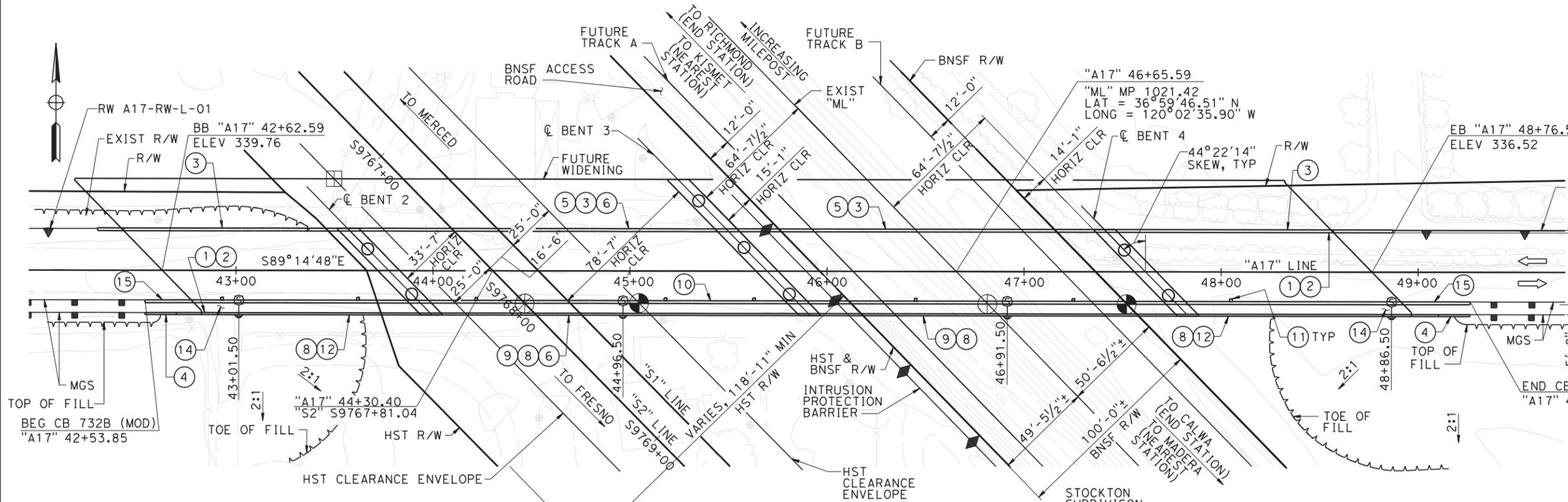
**LEGEND**

- ← DIRECTION OF TRAFFIC
- ⊙ POINT OF MINIMUM VERTICAL CLEARANCE
- ⊕ PERMANENT CLEARANCE ENVELOPE
- ⊗ POINT OF MINIMUM VERTICAL CLEARANCE AT C OF EXIST "ML" TRACK = 27'-5"
- ⊗ POINT OF MINIMUM VERTICAL CLEARANCE AT C OF HST S2 TRACK = 28'-0"
- ⊗ POINT OF MINIMUM VERTICAL CLEARANCE AT C OF HST S2 TRACK = 28'-1" (FUTURE WIDENING)
- ⊙ TYPE 15 LIGHT POLE (FUTURE)
- NS NEAR SIDE
- FS FAR SIDE

**PROFILE GRADE**  
NTS



**ELEVATION**  
1" = 30'  
\* MEASURED NORMAL TO TRACK



**PLAN**  
1" = 30'

**FID: S-OH-168.7**

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DESIGNED BY <b>V. TRINH</b>		CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO. HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2 APPROVED SIGNED _____ DATE _____
DRAWN BY <b>T. DOUNG</b>		
CHECKED BY <b>A. BANESH</b>		
IN CHARGE <b>V. TRINH</b>		
DATE <b>08/30/2019</b>		

0	08/30/19	VT	AB	VT	READY FOR CONSTRUCTION
REV	DATE	BY	CHK	APP	DESCRIPTION

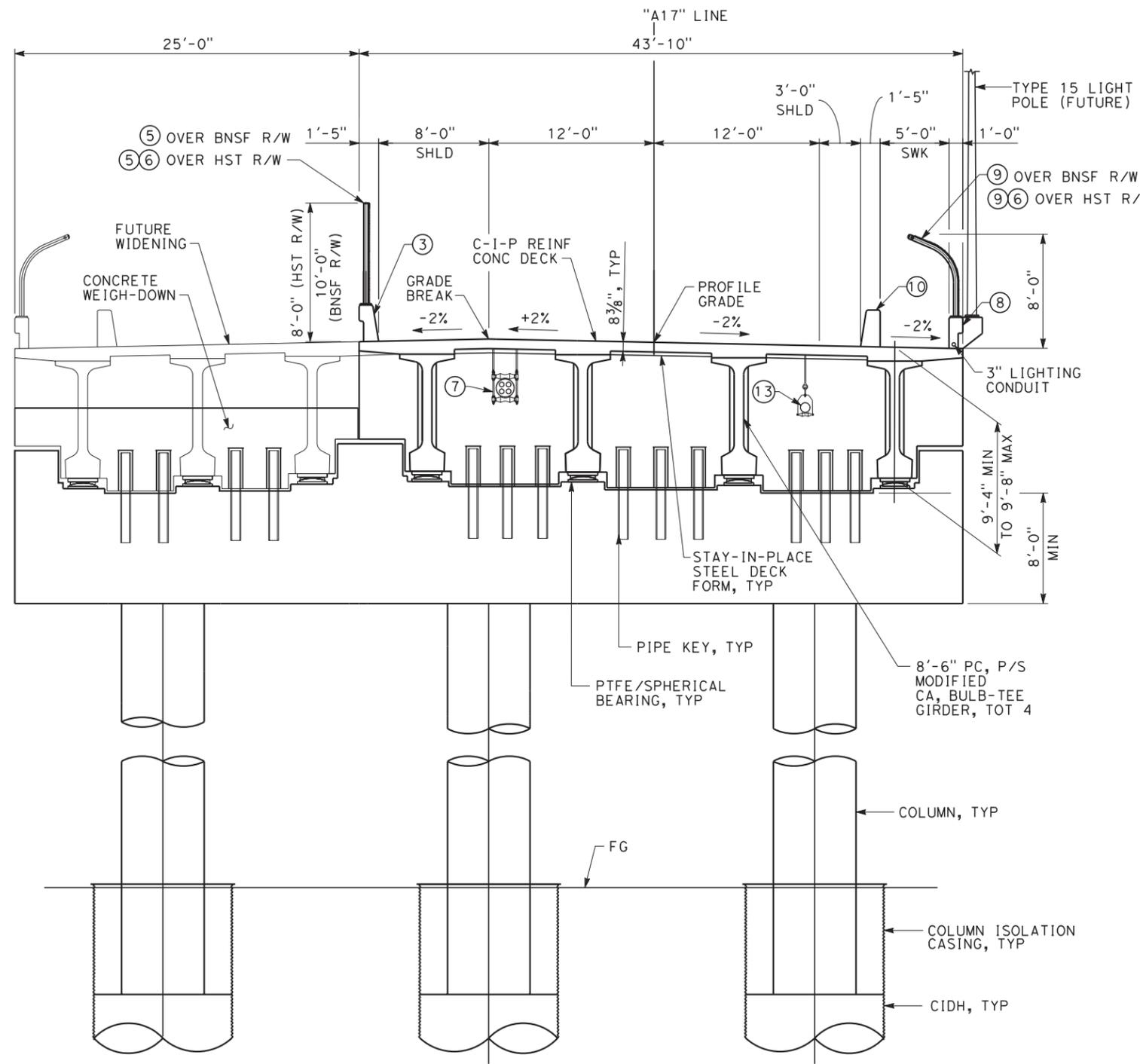
**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

AVENUE 17 OVERHEAD  
GENERAL PLAN AND ELEVATION  
SHEET 1 OF 2

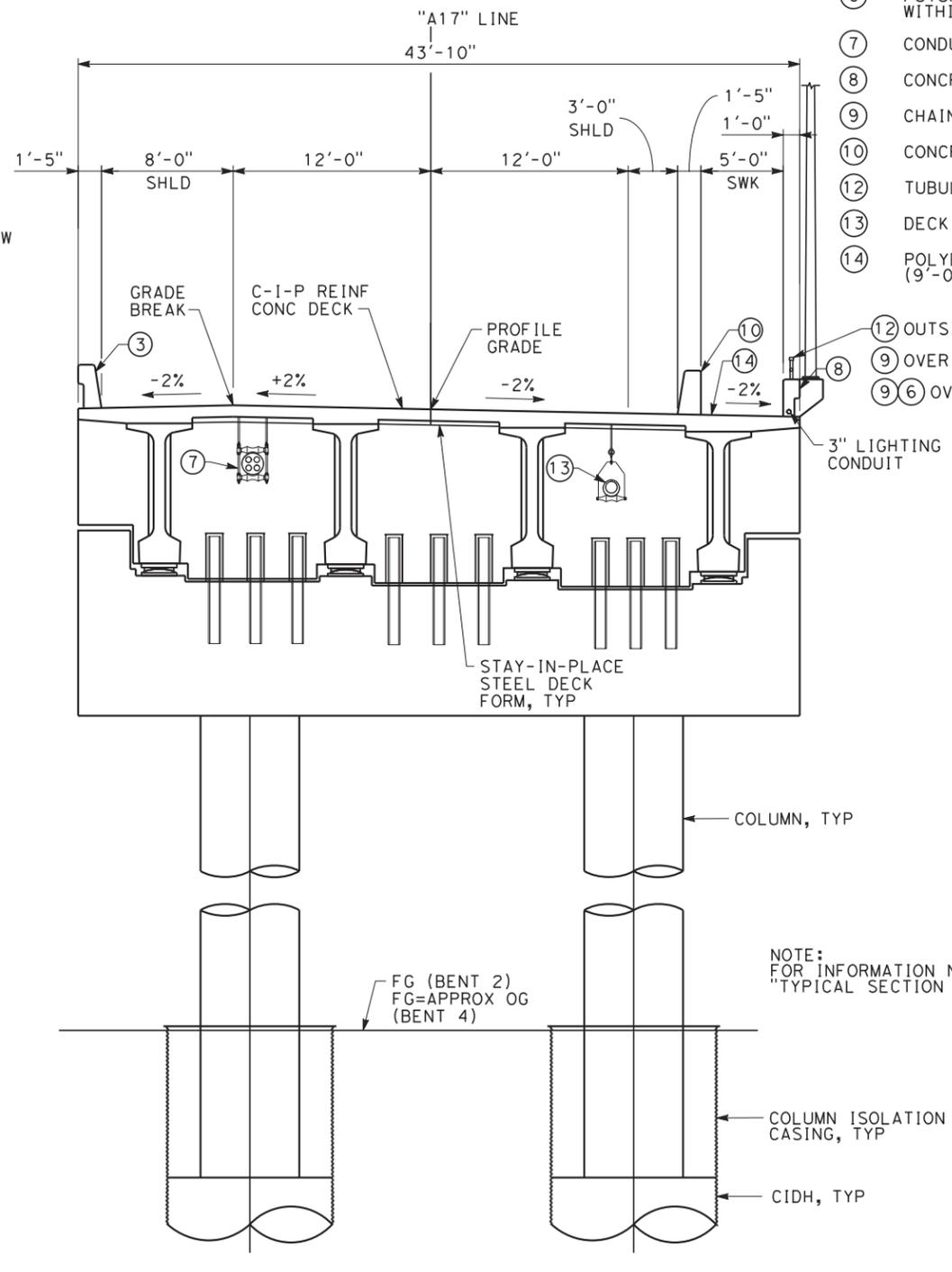
CONTRACT NO. <b>HSR13-06</b>
DRAWING NO. <b>CP1E-ST-K1100-A17</b>
SCALE <b>AS SHOWN</b>
SHEET NO. <b>4</b>

**NOTES:**

- ③ CONCRETE BARRIER TYPE 732
- ⑤ CHAIN LINK RAILING TYPE 7 (MOD)
- ⑥ FUTURE OPAQUE SOLID PLATE (NIC) WITHIN LIMITS OF HST R/W
- ⑦ CONDUIT BUNDLE HANGER
- ⑧ CONCRETE BARRIER TYPE 26 (MOD)
- ⑨ CHAIN LINK RAILING TYPE 3 (MOD)
- ⑩ CONCRETE BARRIER TYPE 732 (MOD)
- ⑫ TUBULAR HAND RAILING
- ⑬ DECK DRAIN PIPE
- ⑭ POLYESTER CONCRETE (9'-0" FROM BB AND EB)



**TYPICAL SECTION - BENT 3**  
1"=5'



**TYPICAL SECTION - BENTS 2 & 4**  
1"=5'

NOTE:  
FOR INFORMATION NOT SHOWN, SEE  
"TYPICAL SECTION - BENT 3".

**FID: S-OH-168.7**

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DESIGNED BY	V. TRINH
DRAWN BY	T. DOUNG
CHECKED BY	A. BANESH
IN CHARGE	V. TRINH
DATE	08/30/2019

0	08/30/19	VT	AB	VT	READY FOR CONSTRUCTION
REV	DATE	BY	CHK	APP	DESCRIPTION

REGISTERED PROFESSIONAL ENGINEER  
VINH DIEU TRINH  
No. C50685  
Exp. 09-30-21  
STATE OF CALIFORNIA

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO. HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2  
APPROVED  
SIGNED \_\_\_\_\_  
DATE \_\_\_\_\_



**CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1 - EXTENSION**

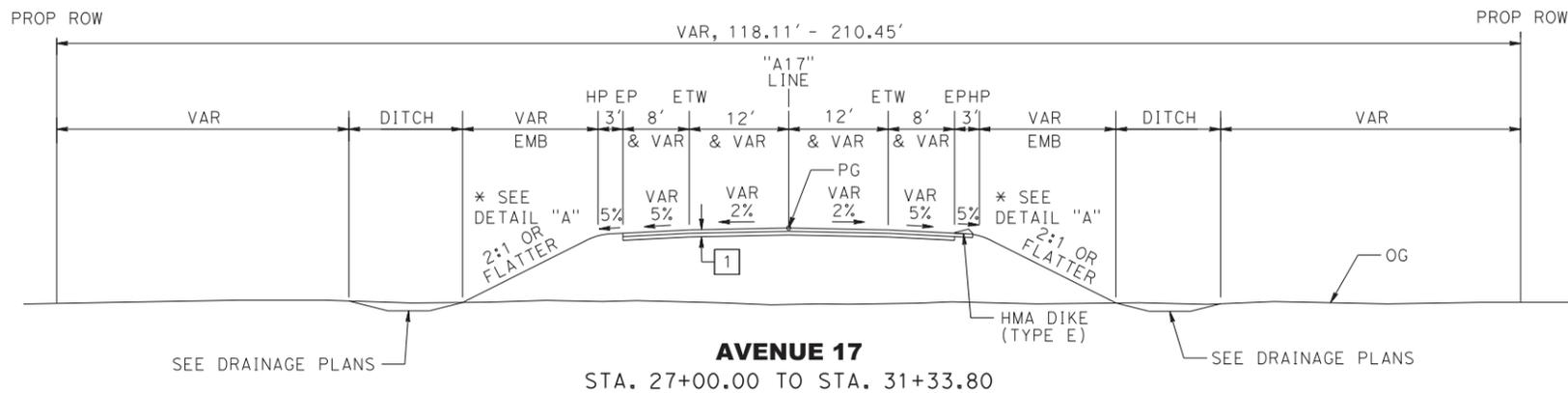
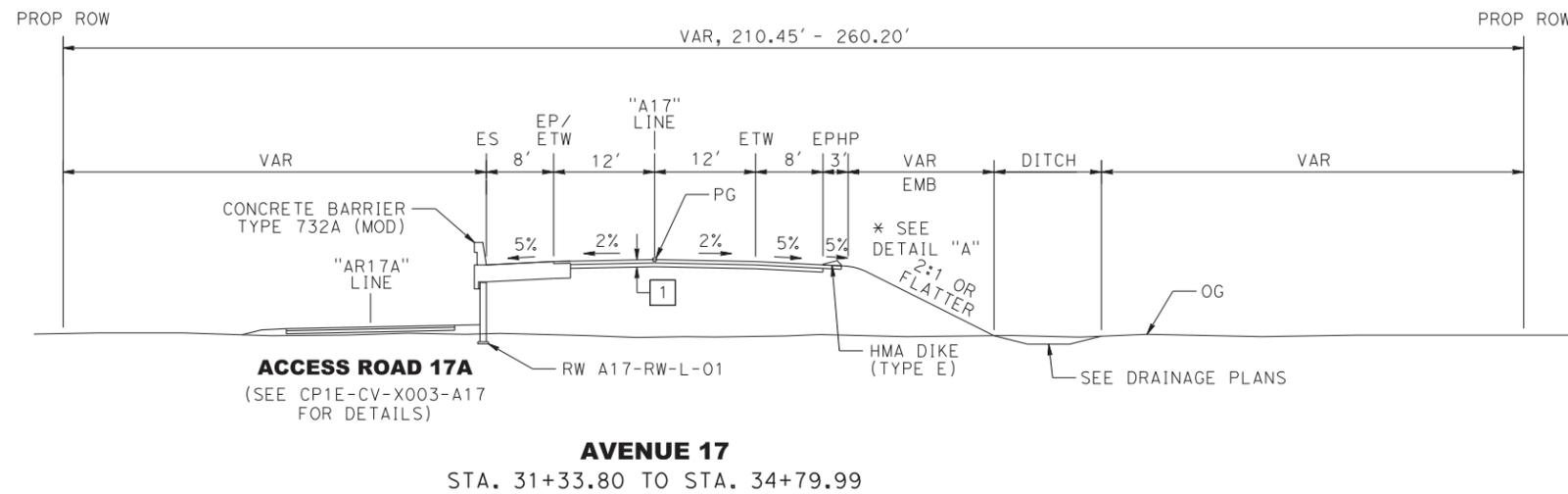
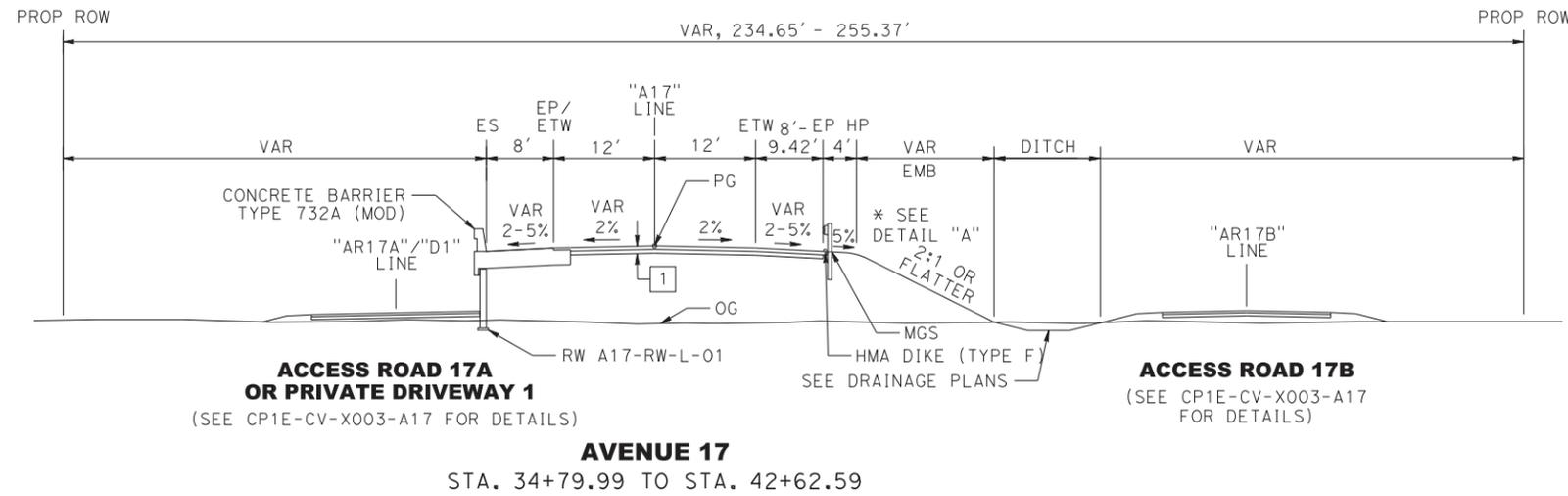
AVENUE 17 OVERHEAD  
GENERAL PLAN AND ELEVATION  
SHEET 2 OF 2

CONTRACT NO.	HSR13-06
DRAWING NO.	CPIE-ST-K1101-A17
SCALE	1"=5'
SHEET NO.	5

**PAVEMENT SECTION**

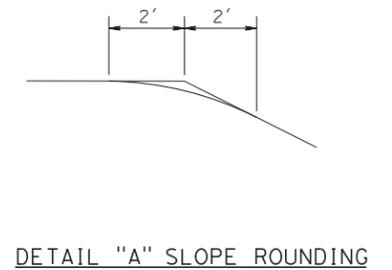
- 1 0.40' HMA-A  
0.45' CLASS 2 AB
- 2 0.25' HMA-A  
0.35' CLASS 2 AB
- 3 0.50' CLASS 2 AB (3/4" MAX)
- 4 0.20' HMA-B  
0.50' CLASS 2 AB

PAVEMENT SECTION APPLICABLE WHEN THE UPPER 4 FEET OF NEW EMBANKMENT MATERIALS (MEASURED FROM THE GRADING PLANE) POSSESS A MINIMUM R-VALUE OF 50 AND A PLASTICITY INDEX OF LESS THAN 12%. WHERE SUBGRADE DOES NOT MEET THESE REQUIREMENTS, SEE GEOTECHNICAL ENGINEERING DESIGN REPORT AVENUE 17 OVERHEAD (NEW) FOR RECOMMENDATIONS.



**NOTES:**

1. FOR SYMBOLS, ABBREVIATIONS, & LEGEND SEE CALTRANS STD PLANS A10A TO A10H.
2. SEE SHEET CP1E-ST-K1101-A17 FOR TYPICAL SECTION STA. 42+62.59 TO STA. 48+76.59.
3. FOR EXISTING PAVEMENT MILLING AND OVERLAY LOCATIONS, SEE LAYOUT PLANS.
4. SEE LAYOUT PLANS FOR MGS AND DIKE LIMITS.
5. EARTHWORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE MADERA COUNTY CONSTRUCTION SPECIFICATIONS, COUNTY OF MADERA SPECIAL PROVISIONS AND THE CONSTRUCTION RECOMMENDATIONS PRESENTED IN GEOTECHNICAL ENGINEERING DESIGN REPORT AVENUE 17 OVERHEAD (NEW).

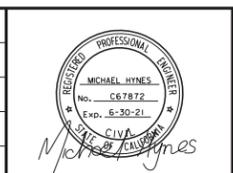


DETAIL "A" SLOPE ROUNDING

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 7/16/2019 3:29:41 PM CHSTP.tbl

DESIGNED BY <b>L. WANG</b>	IN CHARGE <b>M. HYNES</b>
DRAWN BY <b>L. WANG</b>	CHECKED BY <b>M. HYNES</b>
DATE <b>07/19/2019</b>	DATE <b>07/19/2019</b>

REV	DATE	BY	CHK	APP	DESCRIPTION
0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION



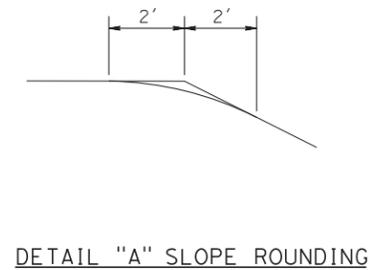
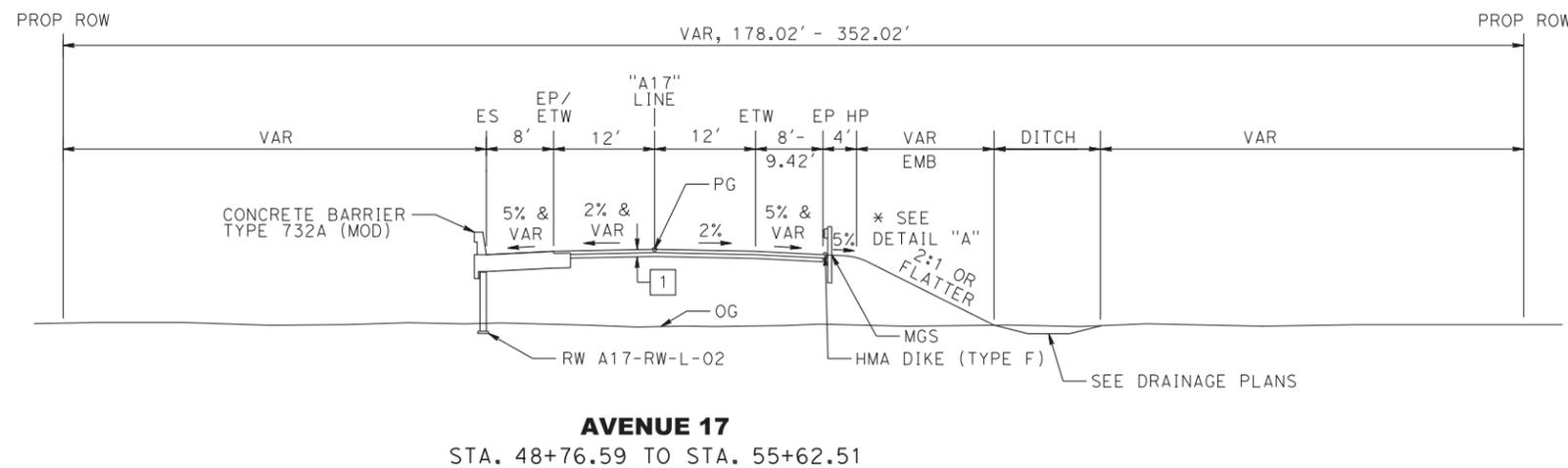
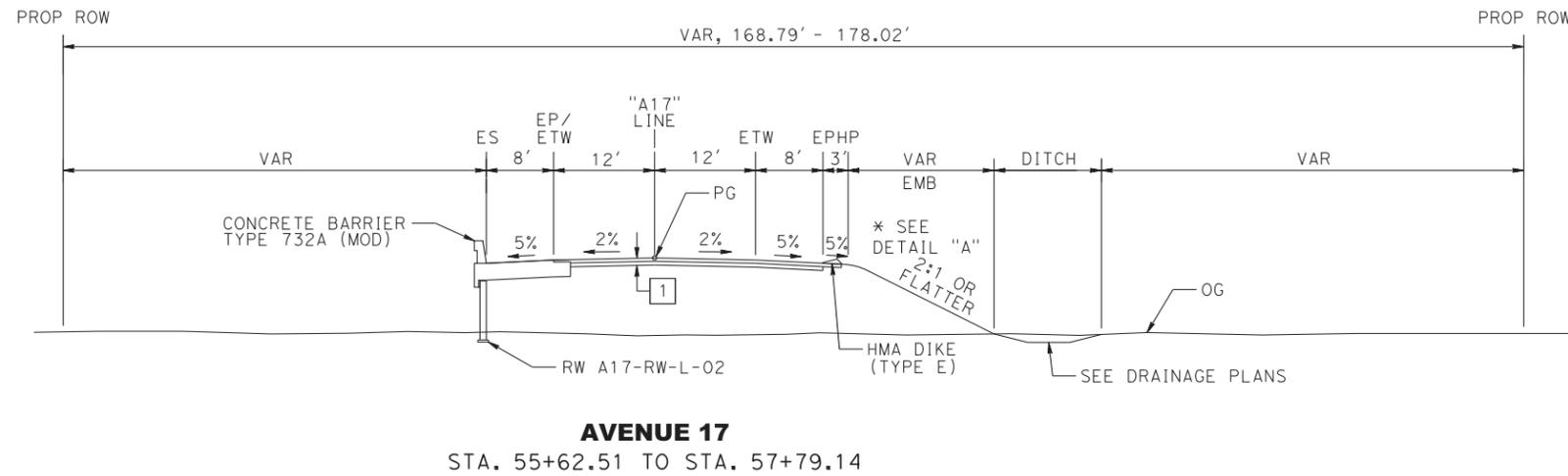
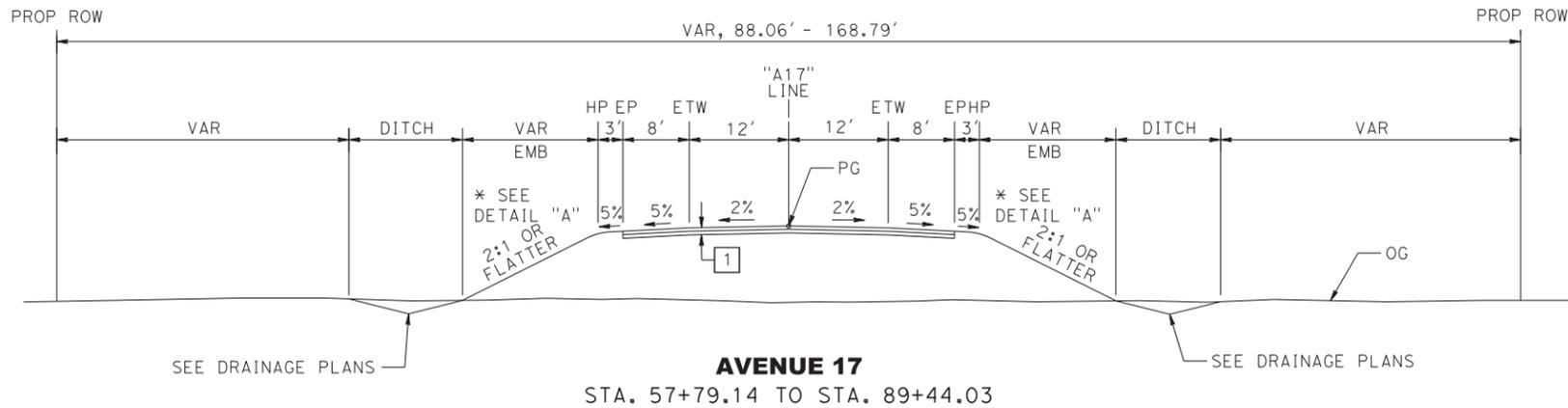
**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**  
  
 AVENUE 17 GRADE SEPARATION  
 TYPICAL SECTIONS  
 SHEET 1 OF 3

CONTRACT NO. HSR13-06
DRAWING NO. CP1E-CV-X001-A17
SCALE NO SCALE
SHEET NO. 3

**NOTE:**  
SEE SHEET CP1E-CV-X001-A17 FOR NOTES.

**PAVEMENT SECTION**

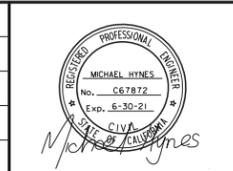
- 1 0.40' HMA-A  
0.45' CLASS 2 AB
- 2 0.25' HMA-A  
0.35' CLASS 2 AB
- 3 0.50' CLASS 2 AB (3/4" MAX)
- 4 0.20' HMA-B  
0.50' CLASS 2 AB



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 7/16/2019 3:29:51 PM CHSTP.tbl CHSTP.tbl

DESIGNED BY	L. WANG
DRAWN BY	L. WANG
CHECKED BY	M. HYNES
IN CHARGE	M. HYNES
DATE	07/19/2019

REV	DATE	BY	CHK	APP	DESCRIPTION
0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION



**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

AVENUE 17 GRADE SEPARATION  
 TYPICAL SECTIONS  
 SHEET 2 OF 3

CONTRACT NO.	HSR13-06
DRAWING NO.	CP1E-CV-X002-A17
SCALE	NO SCALE
SHEET NO.	4

**NOTES:**

- FOR SYMBOLS, ABBREVIATIONS, & LEGEND SEE CALTRANS STD PLANS A10A TO A10H.
- FOR HOT MIX ASPHALT DIKE DETAILS, SEE CALTRANS STD PLANS A87B AND A77N4.
- FOR MIDWEST GUARDRAIL SYSTEM DETAILS, SEE CALTRANS STD PLANS A77P2 AND A77N4.
- FOR MGS TRANSITION RAILING AND CONNECTION DETAILS TO BRIDGE RAILING, SEE CALTRANS STD PLANS A77U1, A77U2, AND A77U4.
- FOR DRIVEWAY AND TURNOUT DETAILS, SEE COUNTY OF MADERA STD PLANS ST-8, ST-25, AND ST-27. USE PAVEMENT SECTION 4. 10' RADIUS ARE USED ON ALL DRIVEWAYS UNLESS OTHERWISE NOTED.

- FOR RETAINING WALL DETAILS, SEE DRAWINGS CP1E-CV-RW001-A17 TO CP1E-CV-RW006-A17.
- FOR RETAINING WALL BARRIER SLAB DETAILS, SEE DRAWING CP1E-CV-RW007-A17.
- FOR DRAINAGE DETAILS, SEE DRAWINGS CP1E-CV-DO01-A17 TO CP1E-CV-DO05-A17, CP1E-CV-DD001-A17 TO CP1E-CV-DD002-A17, AND CP1E-CV-DPO01-A17 TO CP1E-CV-DPO02-A17.
- FOR PAVEMENT DELINEATION & SIGNING DETAILS, SEE DRAWINGS CP1E-CV-PD001-A17 TO CP1E-CV-PD005-A17.
- FOR STRUCTURE DETAILS, SEE DRAWING CP1E-ST-K1100-A17 AND CP1E-ST-K1101-A17.
- HST/MADERA COUNTY ROW TBD BASED ON FINAL AGREEMENT BETWEEN THE AUTHORITY AND MADERA COUNTY.

**DESIGN CRITERIA:**

"A17" AVENUE 17  
 DESIGN SPEED: 55 MPH  
 CLASSIFICATION: PRINCIPAL ARTERIAL ROAD

"AR17A" ACCESS ROAD 17A  
 DESIGN SPEED: 25 MPH  
 CLASSIFICATION: RURAL LOCAL ROAD

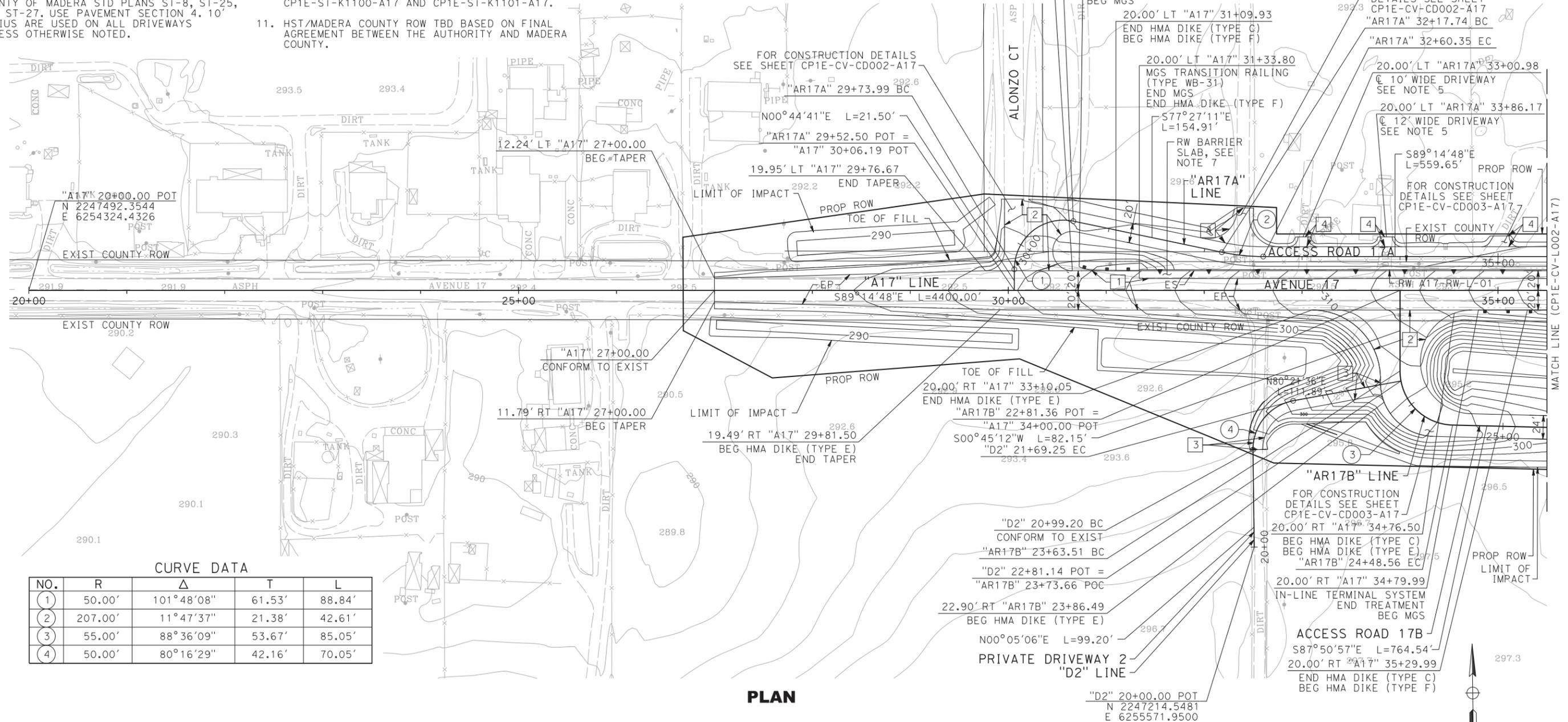
"AR17B" ACCESS ROAD 17B  
 DESIGN SPEED: 25 MPH  
 CLASSIFICATION: RURAL LOCAL ROAD

**LEGEND**

- (X) CURVE NUMBER
- (X) PAVEMENT SECTION

**PAVEMENT SECTION**

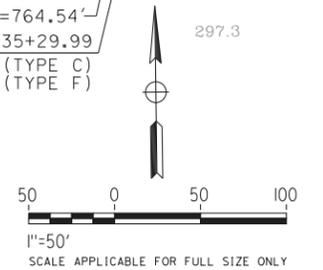
- 1 0.40' HMA-A  
0.45' CLASS 2 AB
- 2 0.25' HMA-A  
0.35' CLASS 2 AB
- 3 0.50' CLASS 2 AB (3/4" MAX)
- 4 0.20' HMA-B  
0.50' CLASS 2 AB



**CURVE DATA**

NO.	R	Δ	T	L
1	50.00'	101°48'08"	61.53'	88.84'
2	207.00'	11°47'37"	21.38'	42.61'
3	55.00'	88°36'09"	53.67'	85.05'
4	50.00'	80°16'29"	42.16'	70.05'

**PLAN**



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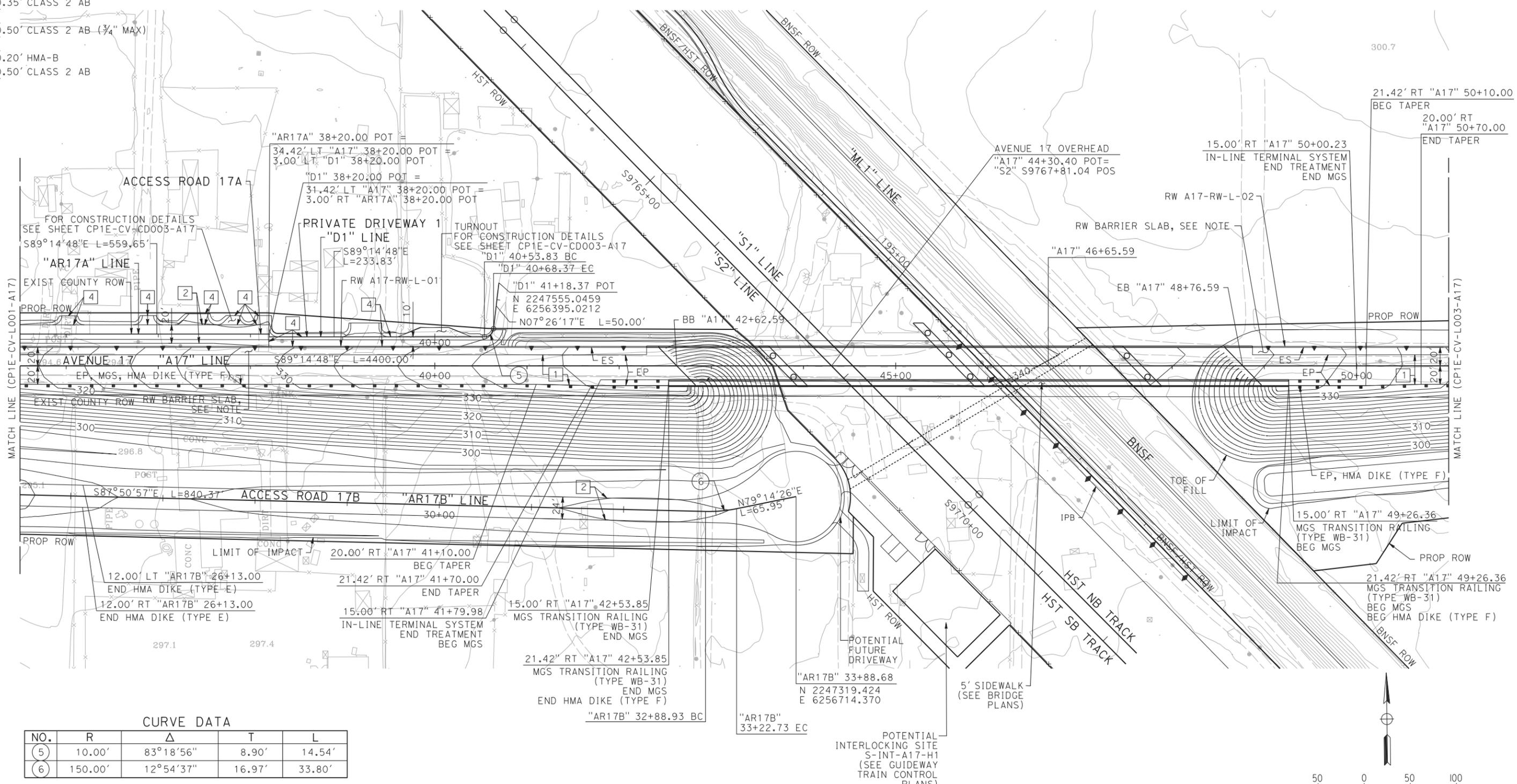
DESIGNED BY <b>L. WANG</b>						<b>CALIFORNIA HIGH-SPEED TRAIN PROJECT</b> <b>CONSTRUCTION PACKAGE 1 - EXTENSION</b>		CONTRACT NO. HSR13-06
DRAWN BY <b>L. WANG</b>								DRAWING NO. CP1E-CV-L001-A17
CHECKED BY <b>M. HYNES</b>								SCALE 1"=50'
IN CHARGE <b>M. HYNES</b>								SHEET NO. 6
DATE <b>07/19/2019</b>								
REV	DATE	BY	CHK	APP	DESCRIPTION	AVENUE 17 GRADE SEPARATION LAYOUT SHEET 1 OF 5		
0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION			

		CONTRACT NO. HSR13-06	
CALIFORNIA HIGH-SPEED RAIL AUTHORITY		DRAWING NO. CP1E-CV-L001-A17	
AVENUE 17 GRADE SEPARATION LAYOUT SHEET 1 OF 5		SCALE 1"=50'	
		SHEET NO. 6	

PAVEMENT SECTION

- 1 0.40' HMA-A  
0.45' CLASS 2 AB
- 2 0.25' HMA-A  
0.35' CLASS 2 AB
- 3 0.50' CLASS 2 AB (3/4" MAX)
- 4 0.20' HMA-B  
0.50' CLASS 2 AB

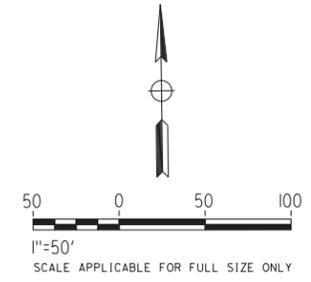
NOTE:  
SEE SHEET CP1E-CV-L001-A17 FOR NOTES.



**CURVE DATA**

NO.	R	Δ	T	L
5	10.00'	83°18'56"	8.90'	14.54'
6	150.00'	12°54'37"	16.97'	33.80'

PLAN



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DESIGNED BY <b>L. WANG</b>	
DRAWN BY <b>L. WANG</b>	
CHECKED BY <b>M. HYNES</b>	
IN CHARGE <b>M. HYNES</b>	
DATE <b>07/19/2019</b>	

REV	DATE	BY	CHK	APP	DESCRIPTION
0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION

A joint venture



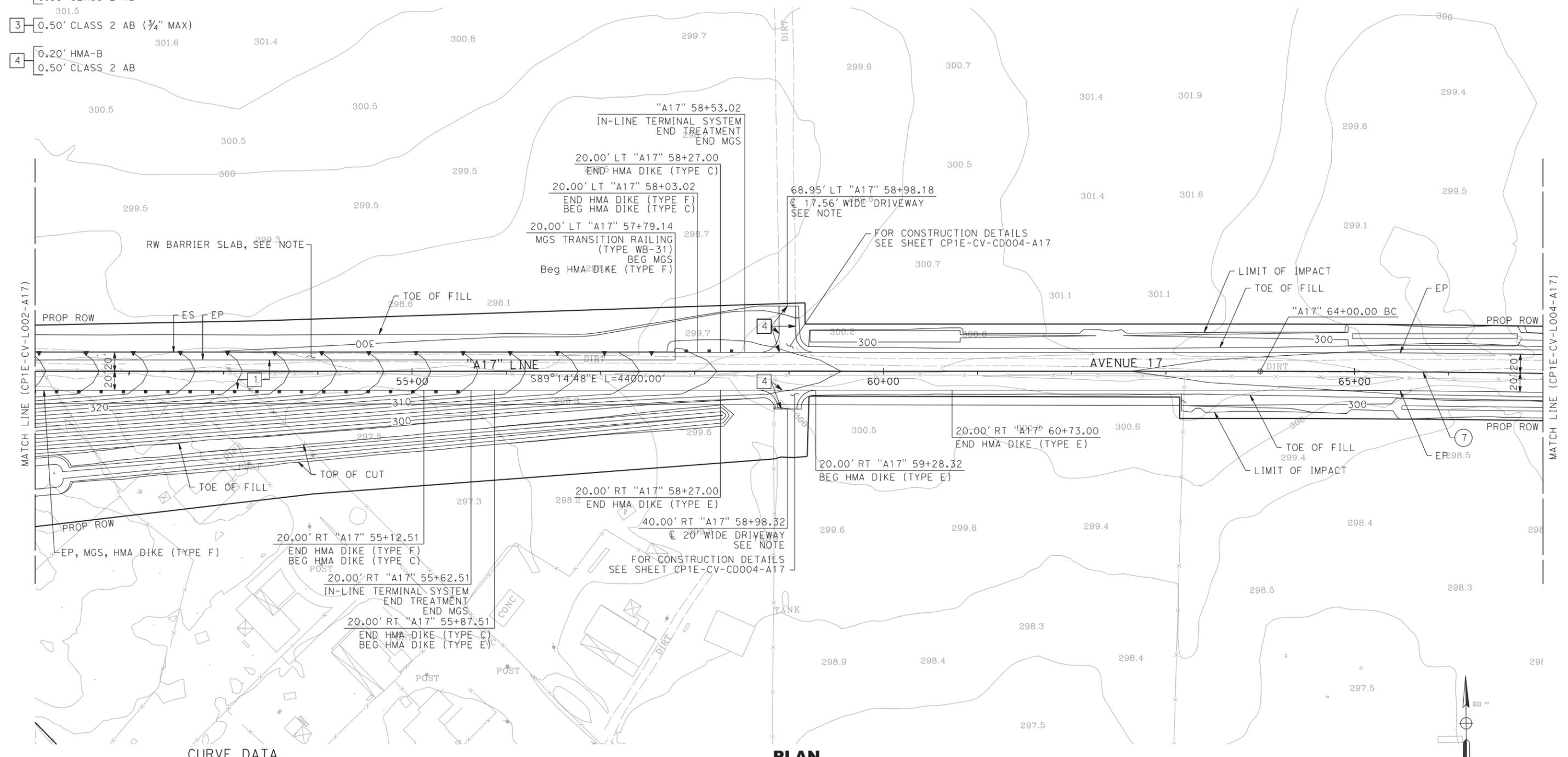
**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**  
 AVENUE 17 GRADE SEPARATION  
 LAYOUT  
 SHEET 2 OF 5

CONTRACT NO. HSR13-06
DRAWING NO. CP1E-CV-L002-A17
SCALE 1"=50'
SHEET NO. 7

PAVEMENT SECTION

- 1 0.40' HMA-A  
0.45' CLASS 2 AB
- 2 0.25' HMA-A  
0.35' CLASS 2 AB  
301.5
- 3 0.50' CLASS 2 AB (¾" MAX)
- 4 0.20' HMA-B  
0.50' CLASS 2 AB

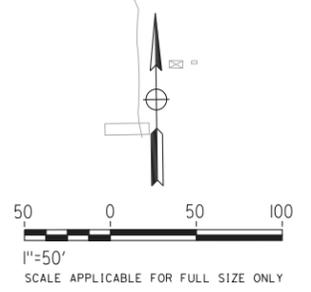
NOTE:  
SEE SHEET CP1E-CV-L001-A17 FOR NOTES.



CURVE DATA

NO.	R	Δ	T	L
7	20000.00'	02°23'41"	418.00'	835.87'

PLAN



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DESIGNED BY  
**L. WANG**  
 DRAWN BY  
**L. WANG**  
 CHECKED BY  
**M. HYNES**  
 IN CHARGE  
**M. HYNES**  
 DATE  
**07/19/2019**



**CALIFORNIA HIGH-SPEED TRAIN PROJECT  
CONSTRUCTION PACKAGE 1 - EXTENSION**

AVENUE 17 GRADE SEPARATION  
 LAYOUT  
 SHEET 3 OF 5

CONTRACT NO.  
HSR13-06  
 DRAWING NO.  
CP1E-CV-L003-A17  
 SCALE  
1"=50'  
 SHEET NO.  
8

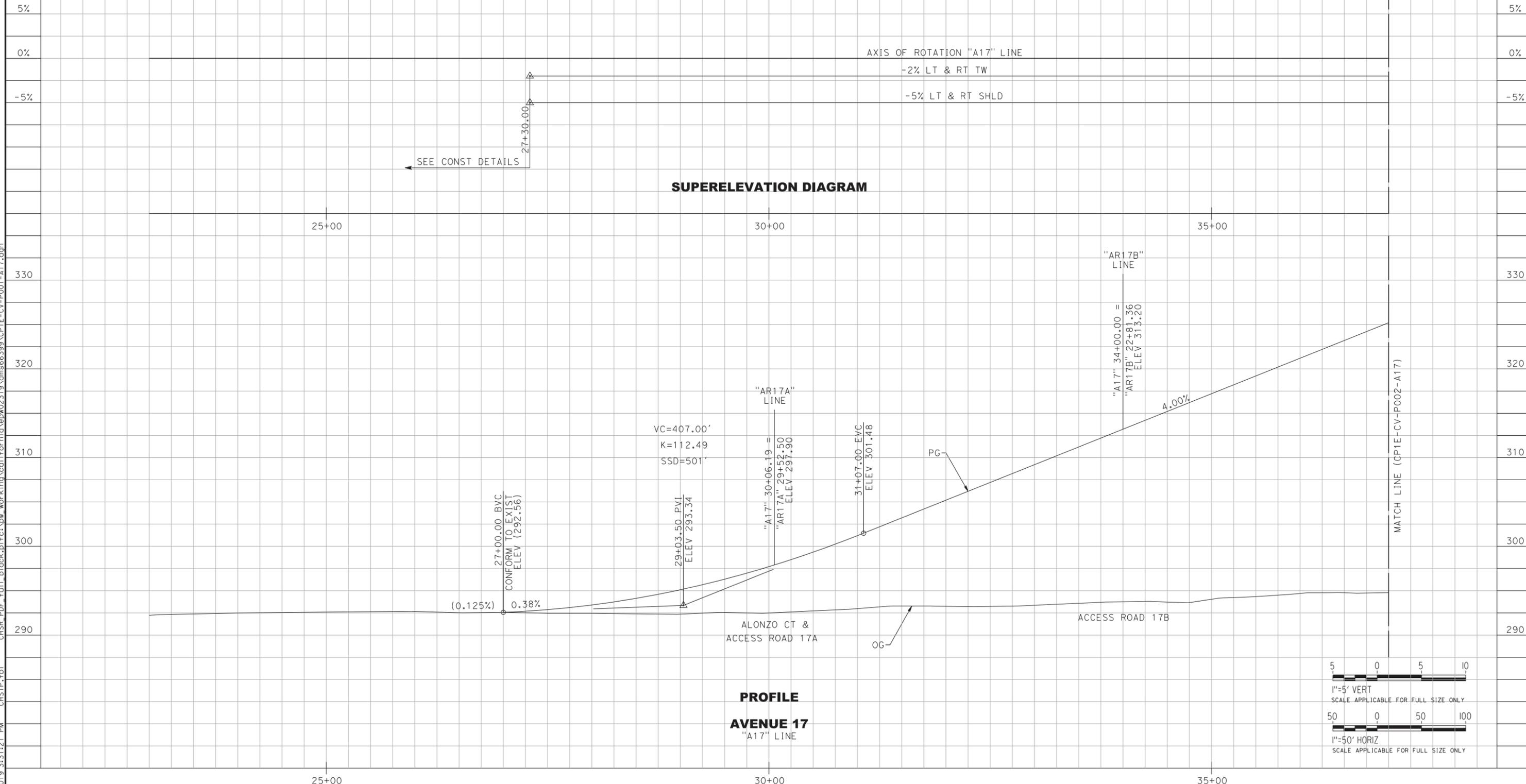
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0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION

**DESIGN CRITERIA:**

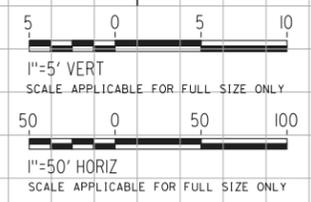
"A17" AVENUE 17  
 DESIGN SPEED: 55 MPH  
 CLASSIFICATION: PRINCIPAL ARTERIAL ROAD

**NOTES:**

- FOR SYMBOLS, ABBREVIATIONS, & LEGEND, SEE CALTRANS STD PLANS A10A TO A10H.
- FOR BRIDGE INFORMATION, SEE STRUCTURE PLANS.



**PROFILE**  
**AVENUE 17**  
 "A17" LINE



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DESIGNED BY	L. WANG
DRAWN BY	L. WANG
CHECKED BY	M. HYNES
IN CHARGE	M. HYNES
DATE	07/19/2019

REV	DATE	BY	CHK	APP	DESCRIPTION
0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION

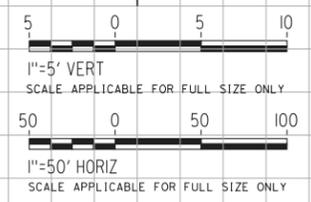
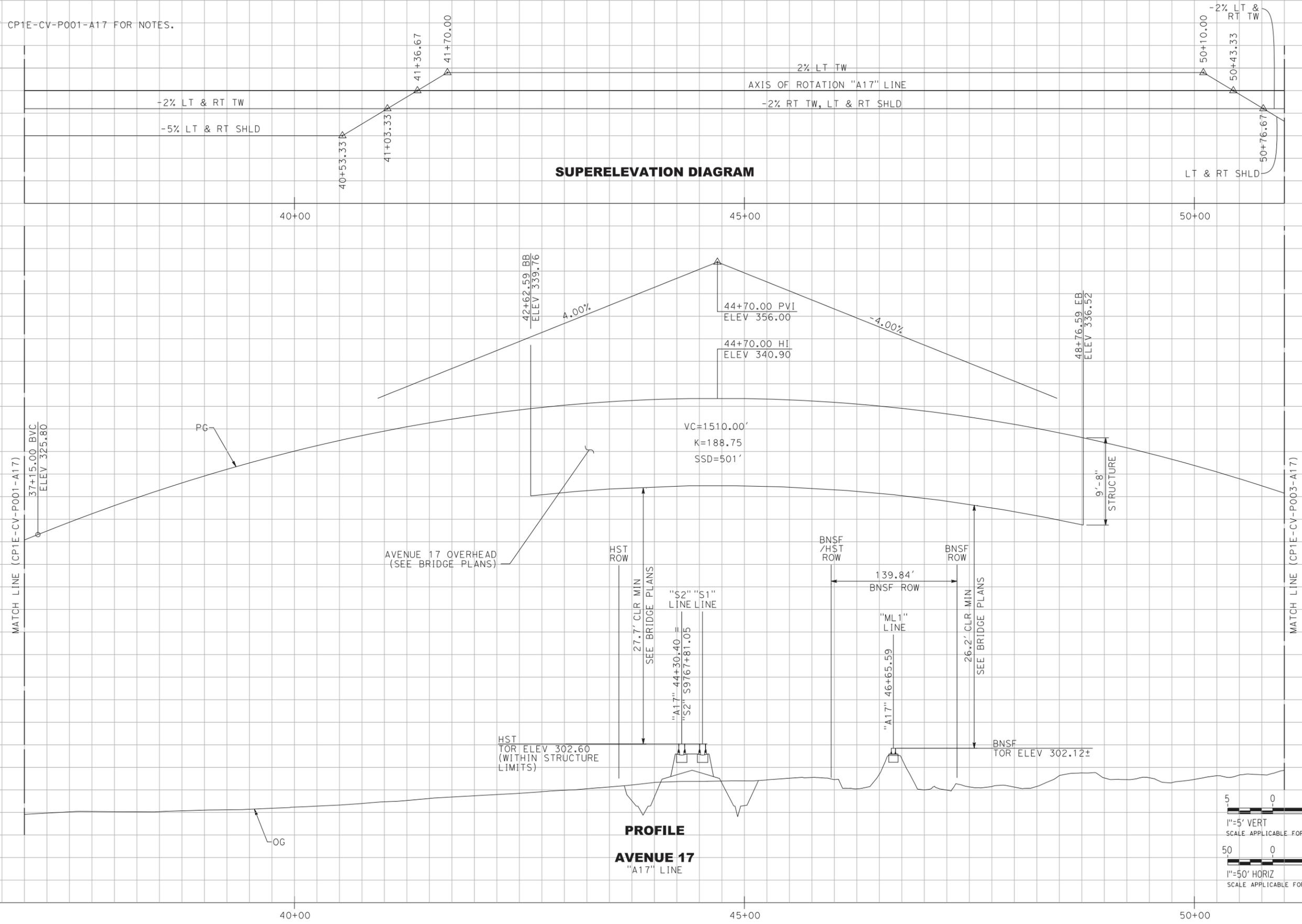


**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

AVENUE 17 GRADE SEPARATION  
 PROFILE AND SUPERELEVATION DIAGRAM  
 SHEET 1 OF 8

CONTRACT NO.	HSR13-06
DRAWING NO.	CP1E-CV-P001-A17
SCALE	AS SHOWN
SHEET NO.	11

**NOTE:**  
SEE SHEET CP1E-CV-P001-A17 FOR NOTES.



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DESIGNED BY <b>L. WANG</b>	CHECKED BY <b>M. HYNES</b>	IN CHARGE <b>M. HYNES</b>	DATE <b>07/19/2019</b>
DRAWN BY <b>L. WANG</b>	DATE <b>07/19/2019</b>	BY <b>LW</b>	APP <b>MH</b>
READY FOR CONSTRUCTION			
REV	DATE	BY	DESCRIPTION
0	07/19/19	LW	MH

DESIGNED BY  
**L. WANG**

DRAWN BY  
**L. WANG**

CHECKED BY  
**M. HYNES**

IN CHARGE  
**M. HYNES**

DATE  
**07/19/2019**

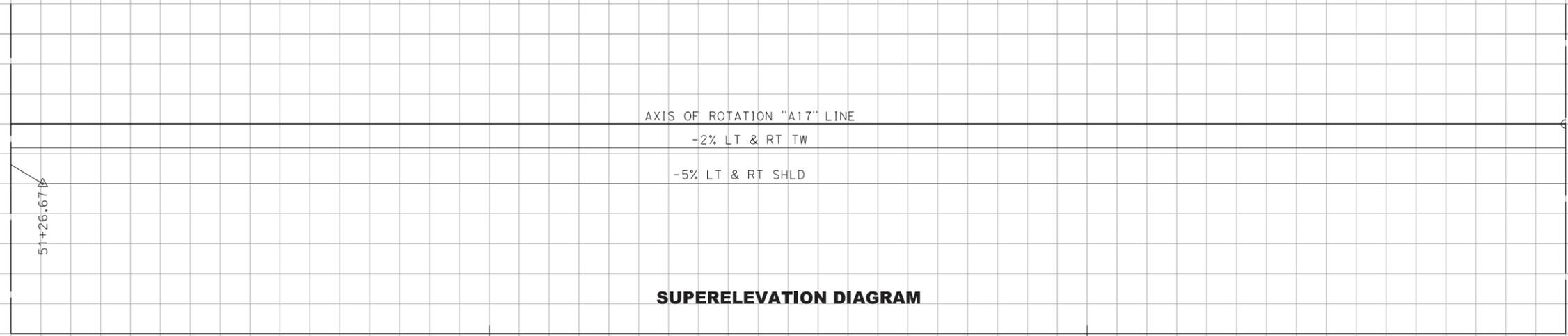


**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

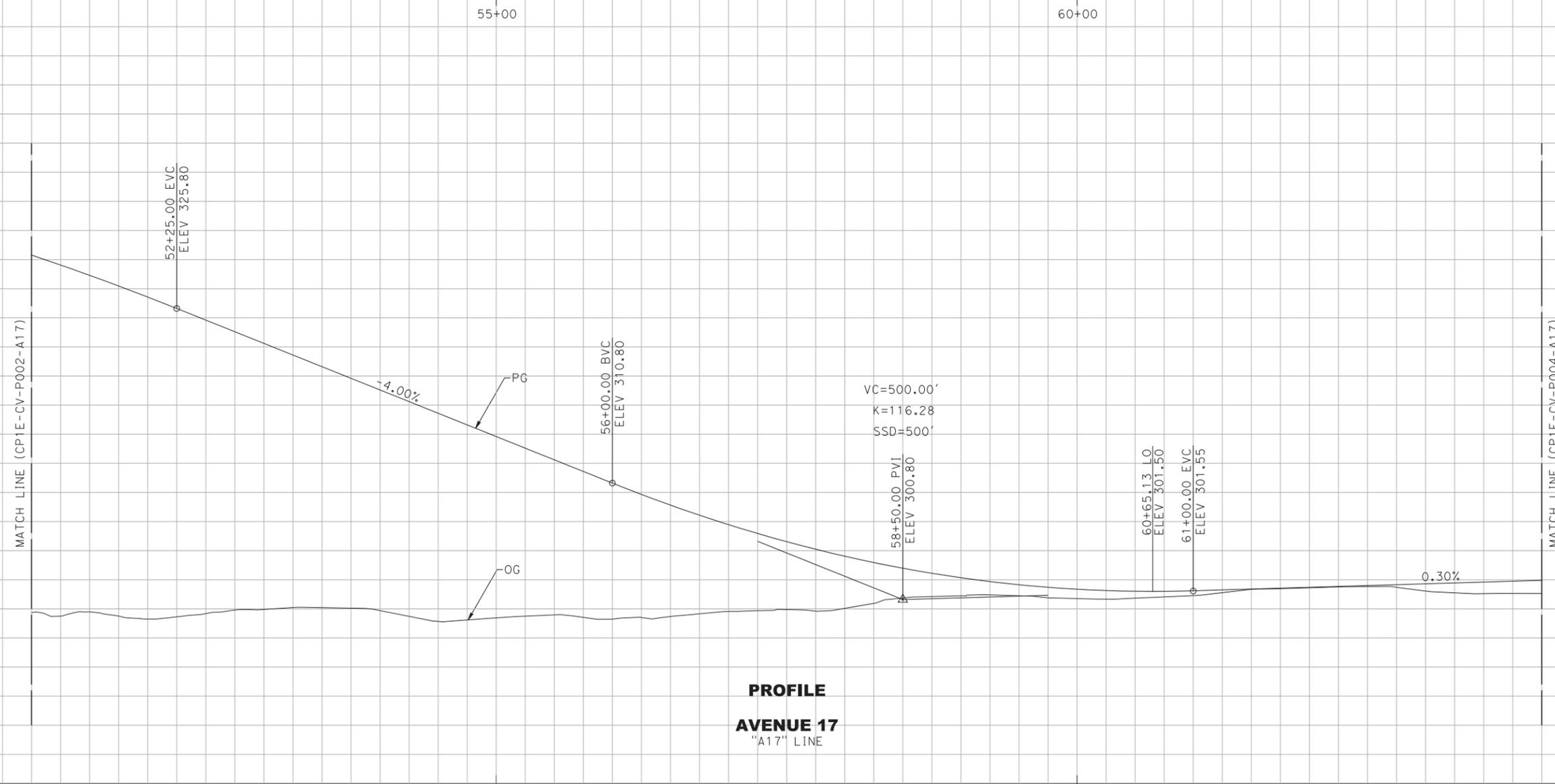
AVENUE 17 GRADE SEPARATION  
PROFILE AND SUPERELEVATION DIAGRAM  
SHEET 2 OF 8

CONTRACT NO. HSR13-06
DRAWING NO. CP1E-CV-P002-A17
SCALE AS SHOWN
SHEET NO. 12

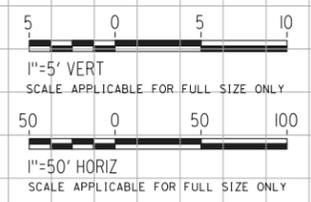
NOTE:  
SEE SHEET CP1E-CV-P001-A17 FOR NOTES.



**SUPERELEVATION DIAGRAM**



**PROFILE**  
**AVENUE 17**  
"A17" LINE



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DESIGNED BY	L. WANG
DRAWN BY	L. WANG
CHECKED BY	M. HYNES
IN CHARGE	M. HYNES
DATE	07/19/2019

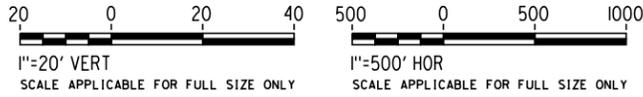
REV	DATE	BY	CHK	APP	DESCRIPTION
0	07/19/19	LW	MH	MH	READY FOR CONSTRUCTION



**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

AVENUE 17 GRADE SEPARATION  
PROFILE AND SUPERELEVATION DIAGRAM  
SHEET 3 OF 8

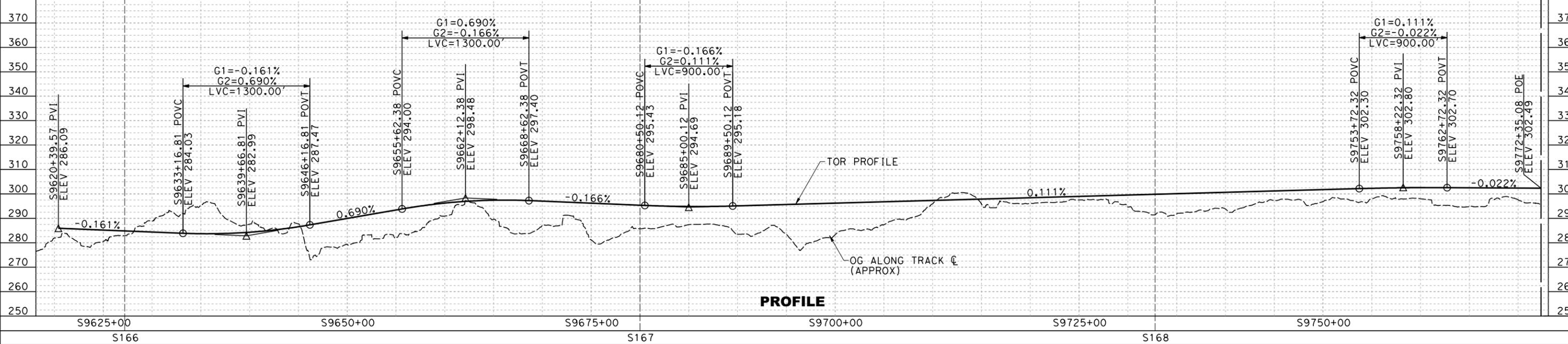
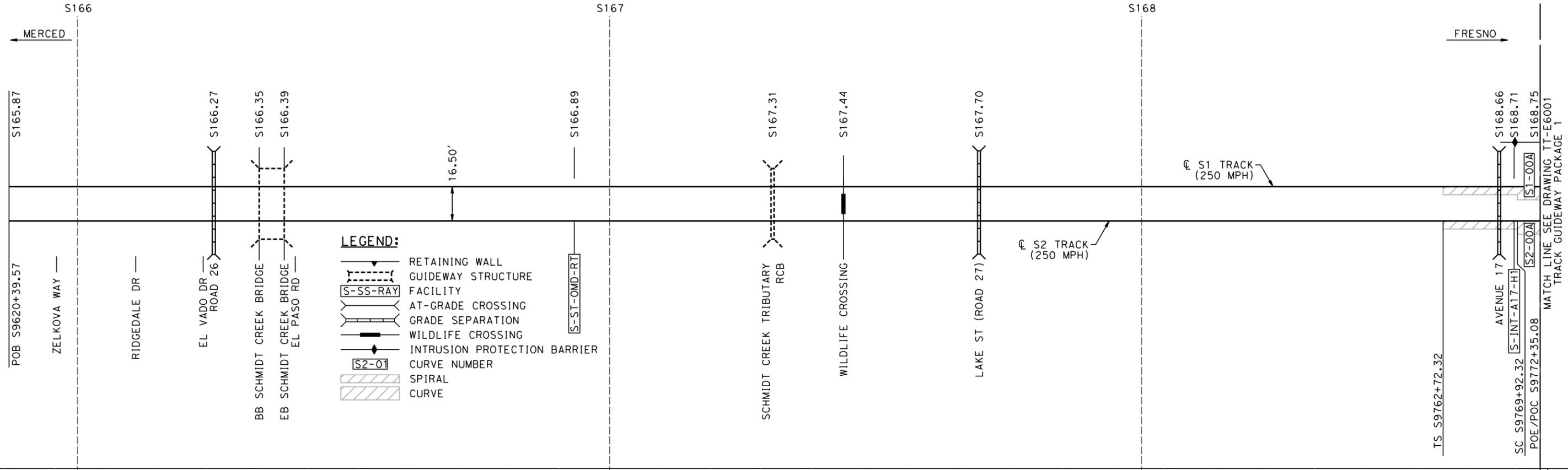
CONTRACT NO.	HSR13-06
DRAWING NO.	CP1E-CV-P003-A17
SCALE	AS SHOWN
SHEET NO.	13



MADERA COUNTY

ALIGNMENT

**NOTE:**  
INFORMATION SHOWN ON TRACK CHART OTHER THAN TRACKWORK SHALL BE USED AS GUIDANCE, AND IS FOR INFORMATION ONLY.



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DESIGNED BY	B. DEMPSTER
DRAWN BY	B. DEMPSTER
CHECKED BY	F. BLACHLY
IN CHARGE	G. NATOLI
DATE	05/29/2019

REV	DATE	BY	CHK	APP	DESCRIPTION
B	05/29/19				90% GUIDEWAY STA 9620+39.57 TO 9772+35.08
A	11/16/18				60% GUIDEWAY STA 9620+39.57 TO 9772+35.08

**NOT FOR CONSTRUCTION**

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2

APPROVED

SIGNED \_\_\_\_\_

DATE \_\_\_\_\_



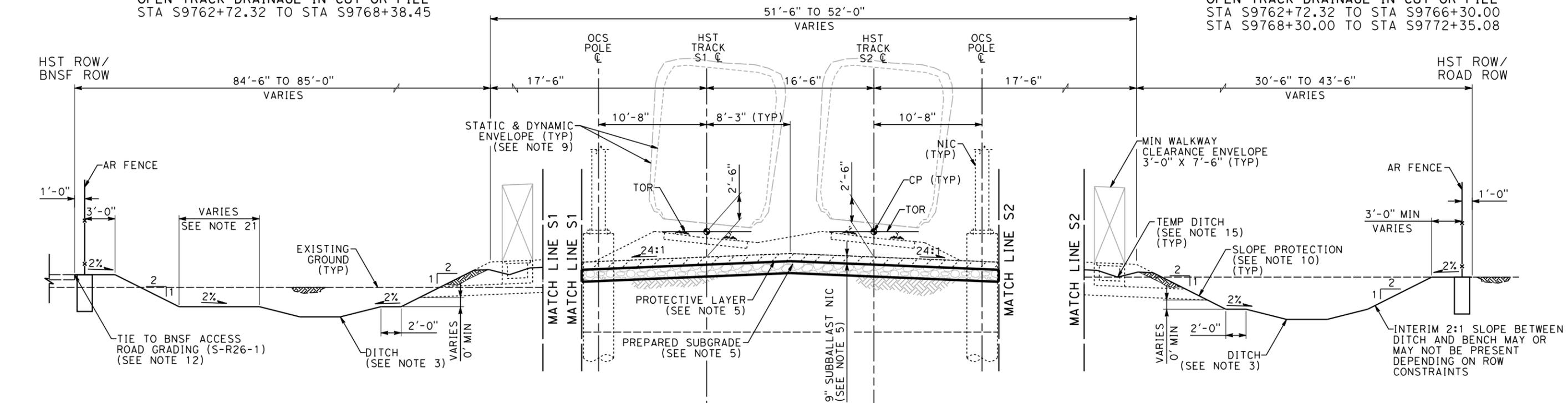
**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

TRACK CHART  
SHEET 1 OF 1

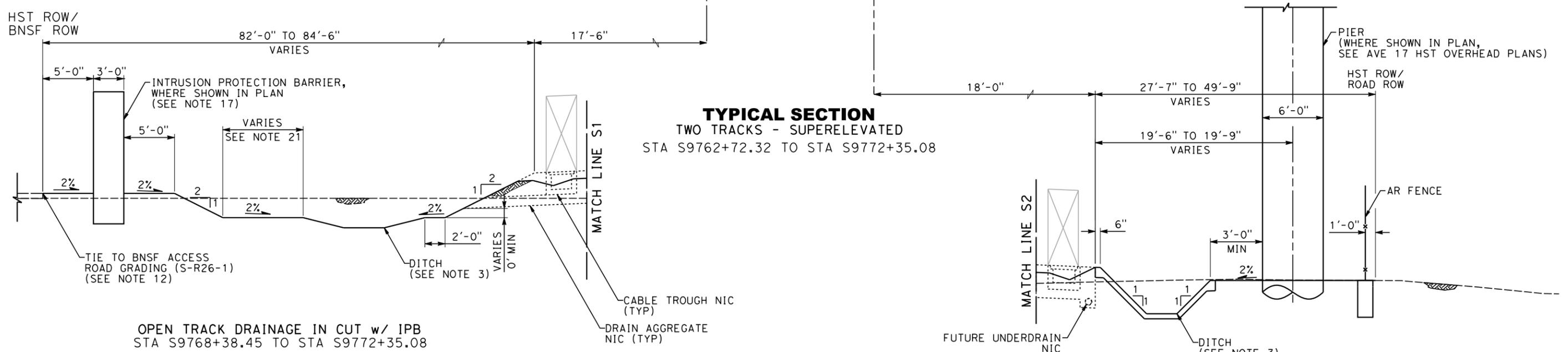
CONTRACT NO.	HSR13-06
DRAWING NO.	CP1E-TT-E6001
SCALE	AS SHOWN
SHEET NO.	

OPEN TRACK DRAINAGE IN CUT OR FILL  
STA S9762+72.32 TO STA S9768+38.45

OPEN TRACK DRAINAGE IN CUT OR FILL  
STA S9762+72.32 TO STA S9766+30.00  
STA S9768+30.00 TO STA S9772+35.08



**TYPICAL SECTION**  
TWO TRACKS - SUPERELEVATED  
STA S9762+72.32 TO STA S9772+35.08



OPEN TRACK DRAINAGE IN CUT w/ IPB  
STA S9768+38.45 TO STA S9772+35.08

CLOSED TRACK DRAINAGE w/ LINED DITCH  
AT AVE 17 OH STRUCTURE  
STA S9766+30.00 TO STA S9768+30.00

NOTE: SEE DWG NO. CP1E-TT-D3001 FOR REFERENCED NOTES.



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 p0018662

DESIGNED BY <b>B. DEMPSTER</b>
DRAWN BY <b>B. DEMPSTER</b>
CHECKED BY <b>F. BLACHLY</b>
IN CHARGE <b>G. NATOLI</b>
DATE <b>05/29/2019</b>

**NOT FOR CONSTRUCTION**

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2

APPROVED

SIGNED \_\_\_\_\_

DATE \_\_\_\_\_

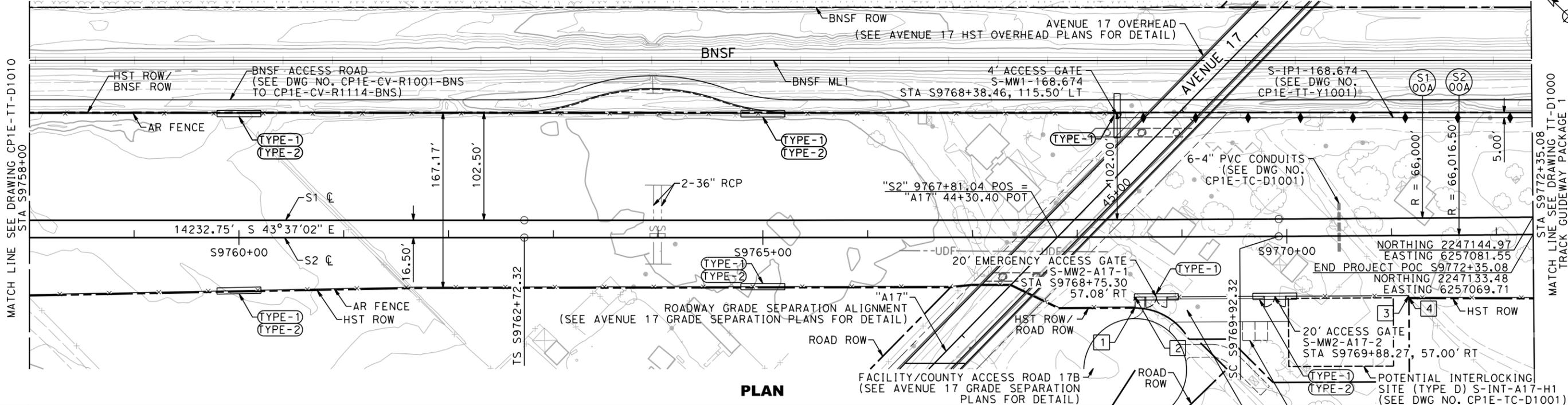


**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**

TRACK TYPICAL SECTION  
SHEET 5 OF 5

CONTRACT NO. HSR13-06
DRAWING NO. CP1E-TT-D3005
SCALE AS SHOWN
SHEET NO.

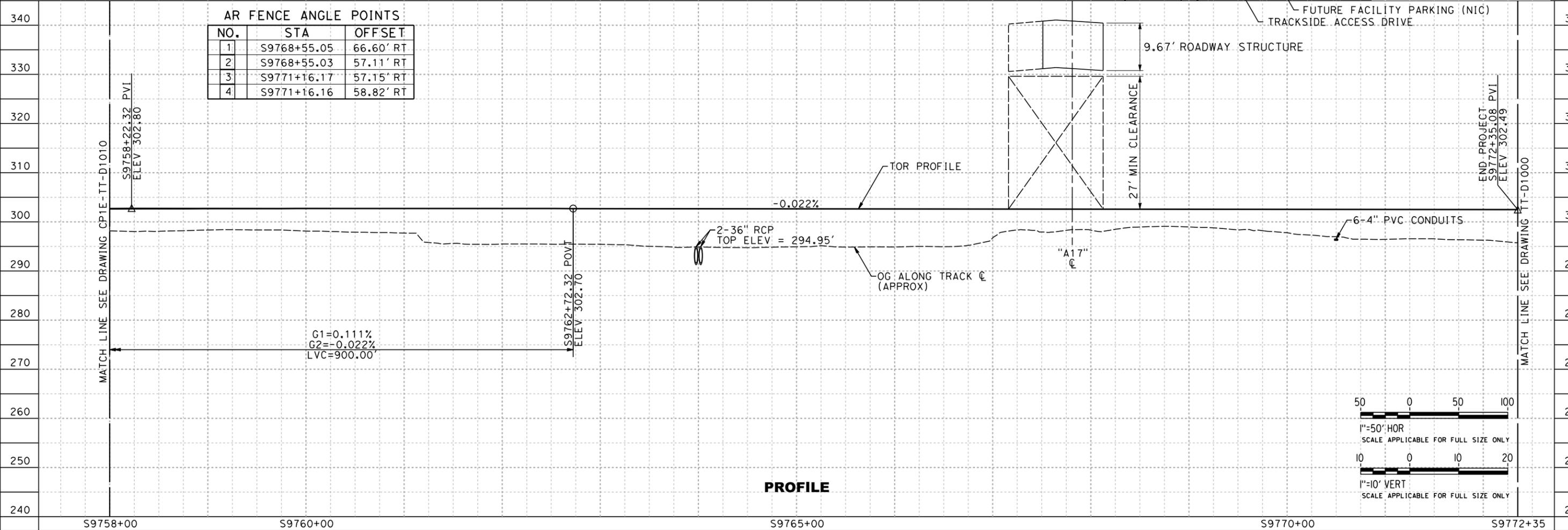
MADERA COUNTY



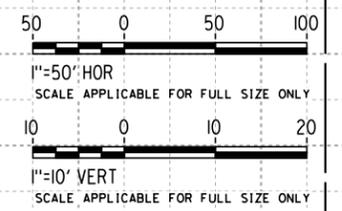
PLAN

AR FENCE ANGLE POINTS

NO.	STA	OFFSET
1	S9768+55.05	66.60' RT
2	S9768+55.03	57.11' RT
3	S9771+16.17	57.15' RT
4	S9771+16.16	58.82' RT



PROFILE



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 7/2/2019 5:25:57 PM

REV	DATE	BY	CHK	APP	DESCRIPTION
B	05/29/19				90% GUIDEWAY STA 9620+39.57 TO 9772+35.08
A	11/16/18				60% GUIDEWAY STA 9620+39.57 TO 9772+35.08

DESIGNED BY  
**B. DEMPSTER**  
 DRAWN BY  
**B. DEMPSTER**  
 CHECKED BY  
**F. BLACHLY**  
 IN CHARGE  
**G. NATOLI**  
 DATE  
**05/29/2019**

**NOT FOR CONSTRUCTION**

CALIFORNIA HIGH-SPEED TRAIN PROJECT CP-1 AGREEMENT NO.4 HSR13-06 SUBMITTAL REVIEW IN ACCORDANCE WITH BOOK 2, PART B - GENERAL PROVISIONS - SECTION 61.2  
 APPROVED  
 SIGNED \_\_\_\_\_  
 DATE \_\_\_\_\_



**CALIFORNIA HIGH-SPEED TRAIN PROJECT**  
**CONSTRUCTION PACKAGE 1 - EXTENSION**  
 TRACK GUIDEWAY  
 STA 9758+00 TO 9772+35.08  
 PLAN AND PROFILE

CONTRACT NO.  
HSR13-06  
 DRAWING NO.  
CP1E-TT-D1011  
 SCALE  
AS SHOWN  
 SHEET NO.

# **Exhibit C**

Local Project Support

BNSF Concurrence



Dionisio Martinez  
Manager Public Projects

BNSF Railway Company  
California Division

740 East Carnegie Dr  
San Bernardino, CA 92408  
Phone: (909) 386-4474

dionisio.martinez@bnsf.com

July 29, 2022

Mr. Marvin Kennix  
Utilities Engineer  
California Public Utilities Commission  
Rail Crossing and Engineering Branch – Safety and Enforcement Division  
180 Promenade Circle, Suite 115  
Sacramento, CA 95834

RE: CHSRA new Overhead grade separation in Madera County at Avenue 17, on the BNSF Stockton Subdivision, MP 1021.42, DOT# 968518J, And CPUC # 002-1021.42-A.

Dear Mr. Kennix,

BNSF Railway (BNSF) is in concurrence with the High-Speed Rail Authority's (Authority) proposed construction of a new highway grade separation at Avenue 17 in Madera County on the BNSF Stockton Subdivision, MP 1021.42.

If you have any questions or concerns, please contact me at 909-386-4474.

A handwritten signature in black ink, appearing to read "Dionisio Martinez". The signature is stylized and cursive.

Dionisio Martinez  
BNSF Railway Company  
Manager Public Projects

# Exhibit D

## Scoping Memo

### Scoping Memo Information for Applications

#### A. Category (Check the category that is most appropriate)

**Adjudicatory** - “Adjudicatory” proceedings are: (1) enforcement investigations into possible violations of any provision of statutory law or order or rule of the Commission; and (2) complaints against regulated entities, including those complaints that challenge the accuracy of a bill, but excluding those complaints that challenge the reasonableness of rates or charges, past, present, or future, such as **formal rough crossing complaints** (maximum 12 month process if hearings are required).

**Ratesetting** - “Ratesetting” proceedings are proceedings in which the Commission sets or investigates rates for a specifically named utility (or utilities), or establishes a mechanism that in turn sets the rates for a specifically named utility (or utilities). “Ratesetting” proceedings include complaints that challenge the reasonableness of rates or charges, past, present, or future. Other proceedings may also be categorized as ratesetting when they do not clearly fit into one category, such as **railroad crossing applications** (maximum 18 month process if hearings are required).

**Quasi-legislative** - “Quasi-legislative” proceedings are proceedings that establish policy or rules (including generic ratemaking policy or rules) affecting a class of regulated entities, including those proceedings in which the Commission investigates rates or practices for an entire regulated industry or class of entities within the industry.

**B. Are hearings necessary?**  **Yes**  **No**

If yes, identify the material disputed factual issues on which hearings should be held, and the general nature of the evidence to be introduced. Railroad crossing applications which are not controversial usually do not require hearings.

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Are public witness hearings necessary?  Yes  No

Public witness hearings are set up for the purpose of getting input from the general public and any entity that will not be a party to the proceeding. Such input usually involves presenting written or oral statements to the presiding officer, not sworn testimony. Public witness statements are not subject to cross-examination.

**C. Issues** - List here the specific issues that need to be addressed in the proceeding.  
None

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**D. Schedule (Even if you checked “No” in B above)** Should the Commission decide to hold hearings, indicate here the proposed schedule for completing the proceeding within 12 months (if categorized as adjudicatory) or 18 months (if categorized as ratesetting or quasi-legislative).

The schedule should include proposed dates for the following events as needed:

<u>Tbd</u>	Filing Date
<u>Tbd</u>	Comment Period
<u>Tbd</u>	Proposed Decision (4 months from filing)
<u>Tbd</u>	Final Decision (6 months from filing)

If hearings unexpectedly becomes necessary:

<u>Tbd</u>	Prehearing conference
<u>Tbd</u>	Hearings
<u>Tbd</u>	Briefs due
<u>Tbd</u>	Submission
<u>Tbd</u>	Proposed decision (90 days after submission)
<u>Tbd</u>	Final decision (60 days after proposed decision is mailed)

# Exhibit E

## Verification

I am an officer of the California High Speed Rail Authority, Applicant herein, and am authorized to make this verification on its behalf. The contents of this document are true of my own knowledge, except as matters that are stated on information or belief, and as to those matters, I believe them to be true.

Dated this 12th day of February, 2025 at Sacramento, California.

Signed \_\_\_\_\_

A black and white image of a handwritten signature in cursive script, appearing to read 'D. Rulens', set against a solid black rectangular background.

Dominique Rulens  
Acting Director of Rail and  
Operations California High-Speed  
Rail Authority 770 L Street, Suite 620  
Sacramento, CA 95814  
Tel: (916) 908-1277

**BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA**

Application of the California High-Speed Rail Authority to construct an overhead grade separation at Avenue 17 over BNSF's existing track (MP 1021.42); and proposed high-speed tracks (MP 168.67) within Madera County, California.

Application No. \_\_\_\_\_

**NOTICE OF AVAILABILITY**

**EXHIBIT C2 - MASTER AGREEMENT**

**EXHIBIT C3 - SETTLEMENT AGREEMENT**

**EXHIBIT D - FINAL ENVIRONMENTAL IMPACT REPORT/ ENVIRONMENTAL IMPACT STATEMENT FOR THE MERCED TO FRESNO SECTION OF THE CALIFORNIA HIGH-SPEED TRAIN PROJECT**

TO ALL PARTIES TO THIS APPLICATION:

In support of its applications, the California High-Speed Rail Authority (Applicant) submitted Exhibit C2 *Master Agreement*, Exhibit C3 *Settlement Agreement*, and Exhibit D *Final Environmental Impact Report/ Environmental Impact Statement for the Merced to Fresno Section of the California High-Speed Train Project (Final EIR/EIS)* to the Commission as a part of Application Number A.15-10-007.

Exhibits C2 and C3 were filed electronically with the Docket Office. Exhibit D *Final EIR/EIS* was provided to the Docket Office on an archival and standard CD-ROM/DVDs for physical filing.

Pursuant to Rule 1.9(d) of the Commission's Rules of Practice and Procedure, the Applicant is issuing this Notice of Availability (NOA). The NOA is being served on

all parties listed on the official service lists for this application. See the accompanying Certificate of Service.

Exhibit C2 *Master Agreement* and Exhibit C3 *Settlement Agreement* to Application A. 15-10-007 are available at the following URL, where they have been posted since October 2015

<http://docs.cpuc.ca.gov/PublishedDocs/Efile/G000/M155/K377/155377211.PDF> ,.

Exhibit D *Final EIR/EIS*, which has been posted at the Applicant's web-site since late 2012, is available at the following URL:

<https://hsr.ca.gov/programs/environmental-planning/project-section-environmental-documents-tier-2/merced-to-fresno-final-environmental-impact-report-statement-eir-s/>

**California High-Speed Rail Authority**  
Dominique Rulens  
Acting Director of Rail and Operations  
770 L Street, Suite 620  
Sacramento, CA 95814  
(916) 908-1277  
[Dominique.Rulens@hsr.ca.gov](mailto:Dominique.Rulens@hsr.ca.gov)